

COMMITTEE ROOM



Lloyd's Register

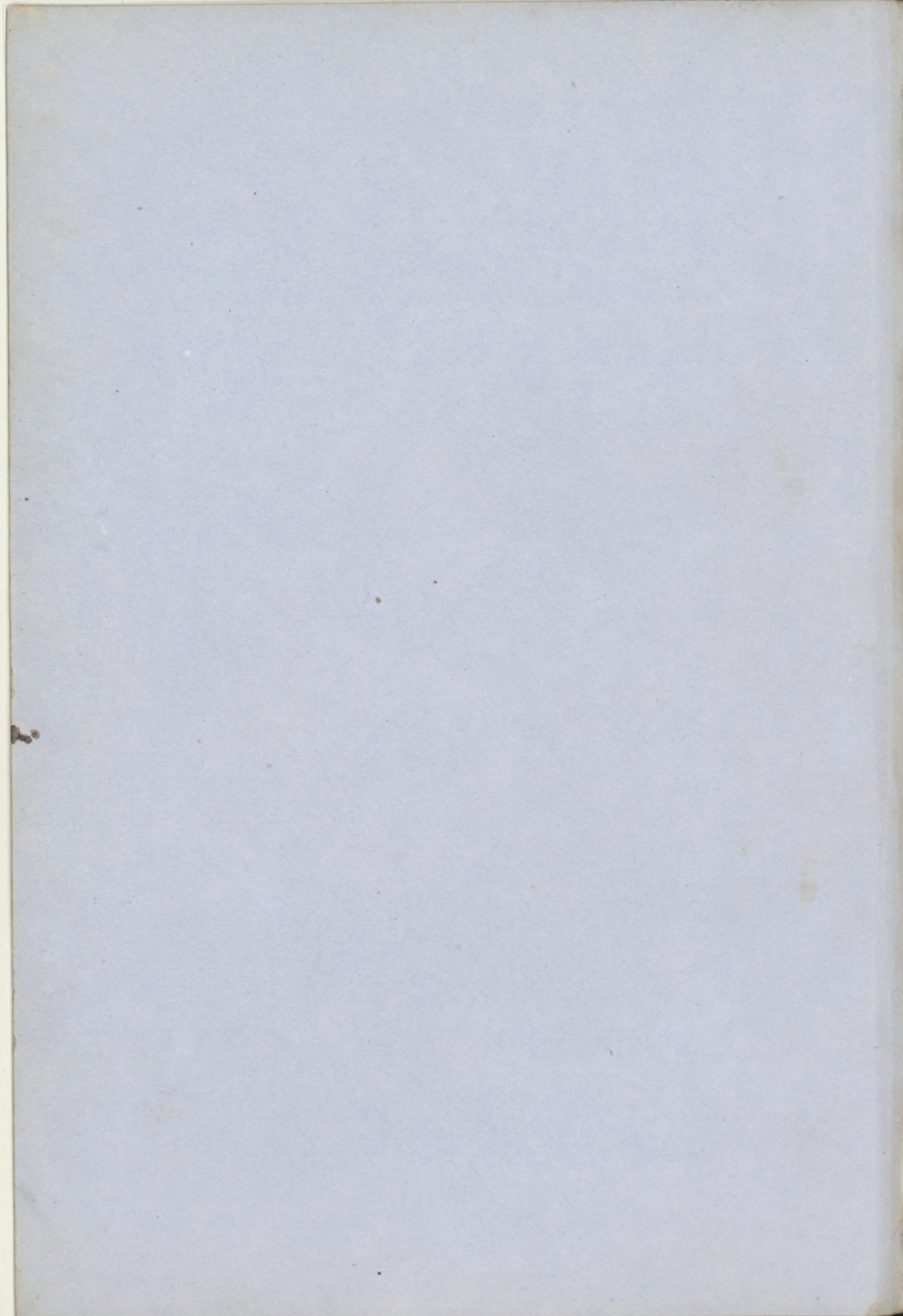
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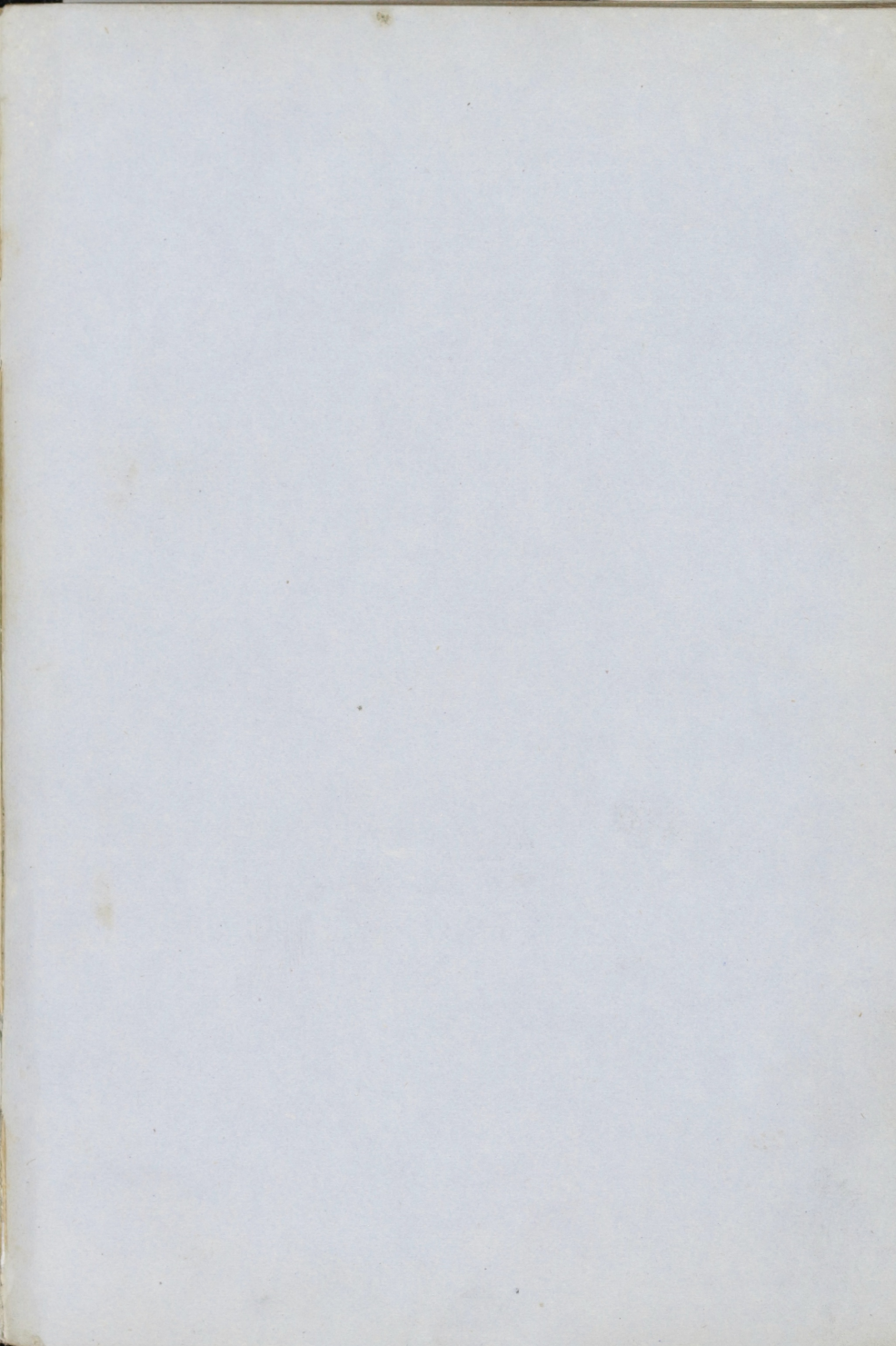
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LLOYD'S REGISTER
OF
BRITISH AND FOREIGN
SHIPPING.

THE YOUNG MAN

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LLOYD'S REGISTER
OF
BRITISH AND FOREIGN
SHIPPING.

From 1st JULY, 1862, to the 30th JUNE, 1863.

ESTABLISHED 1834.



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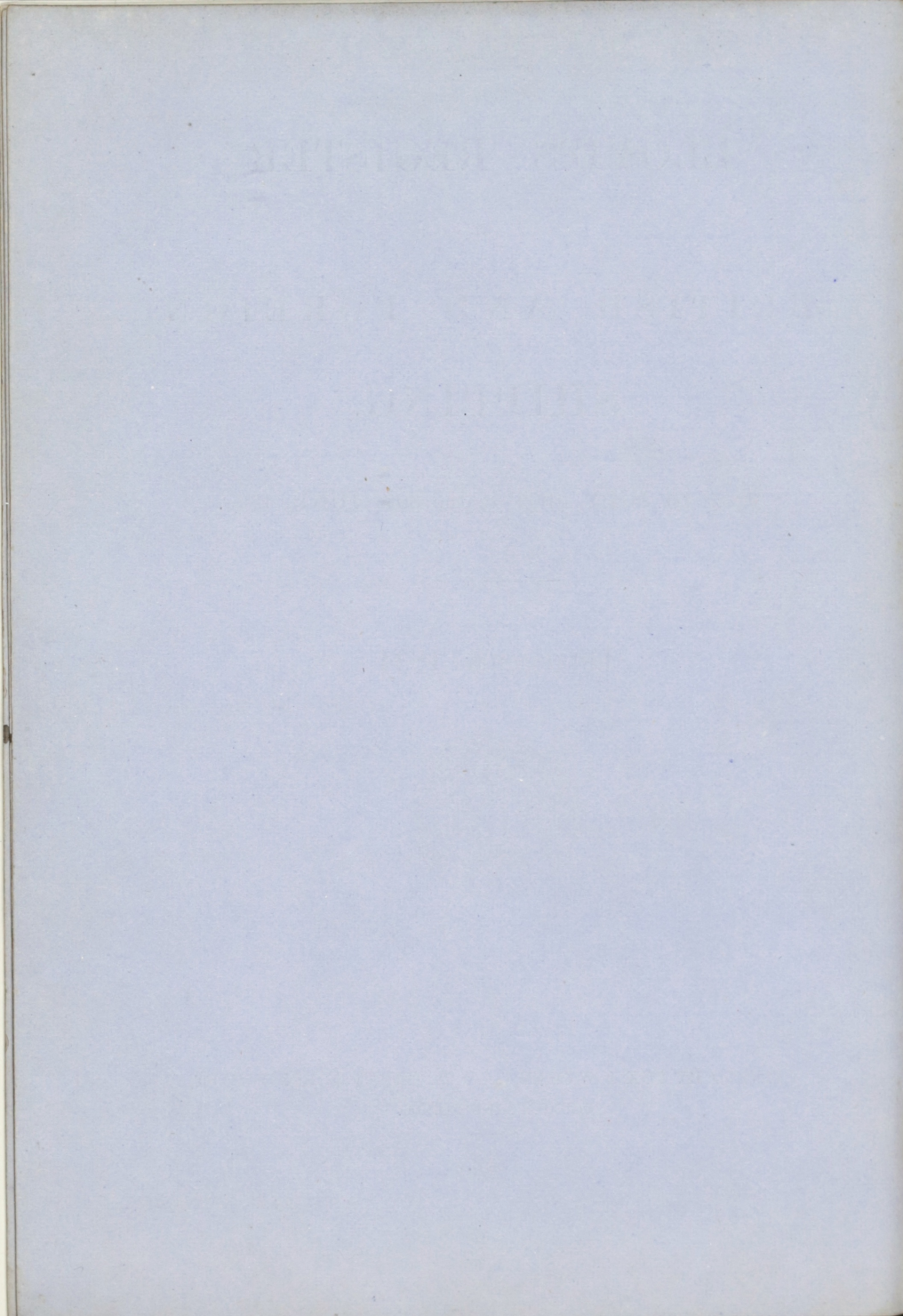
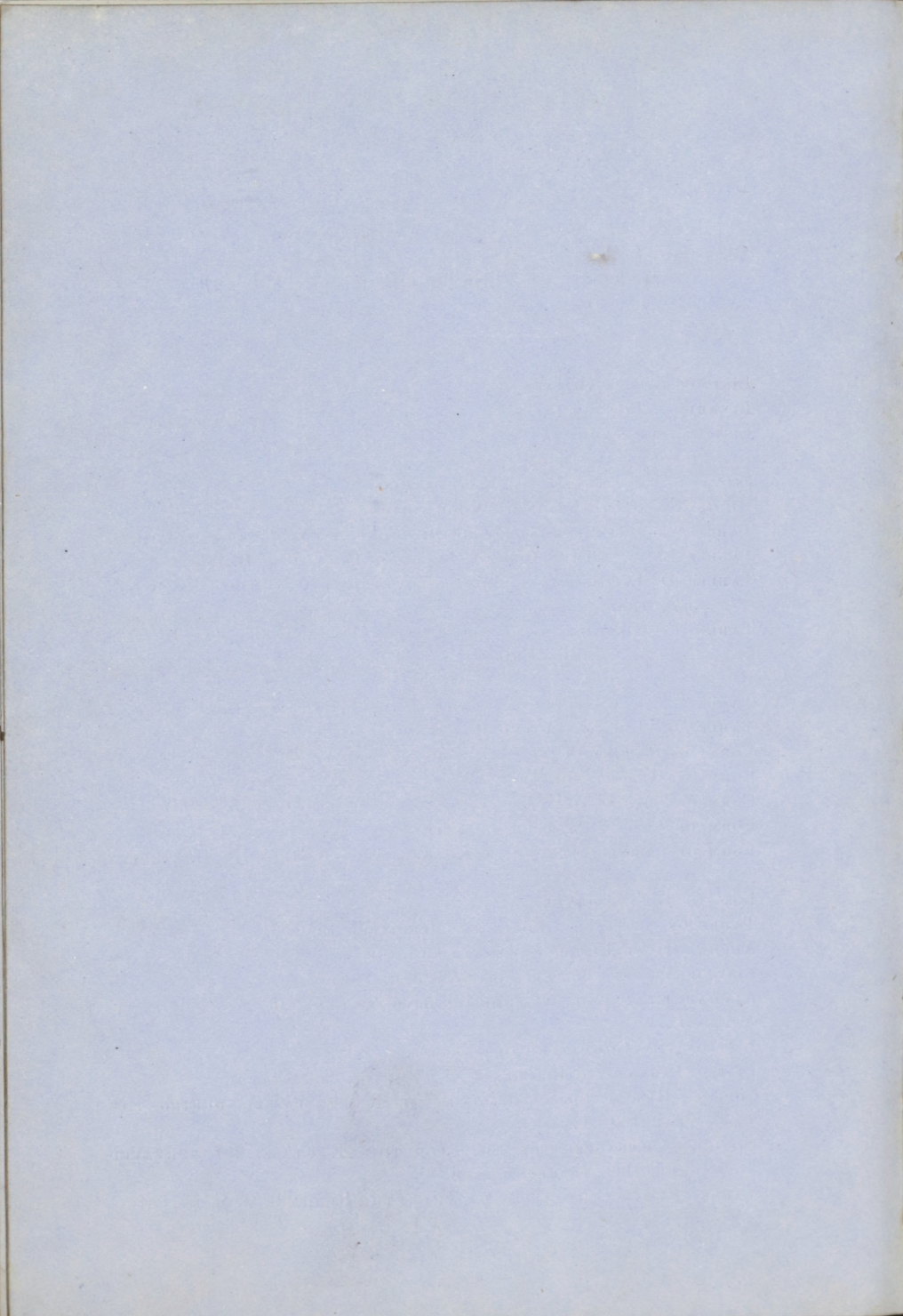


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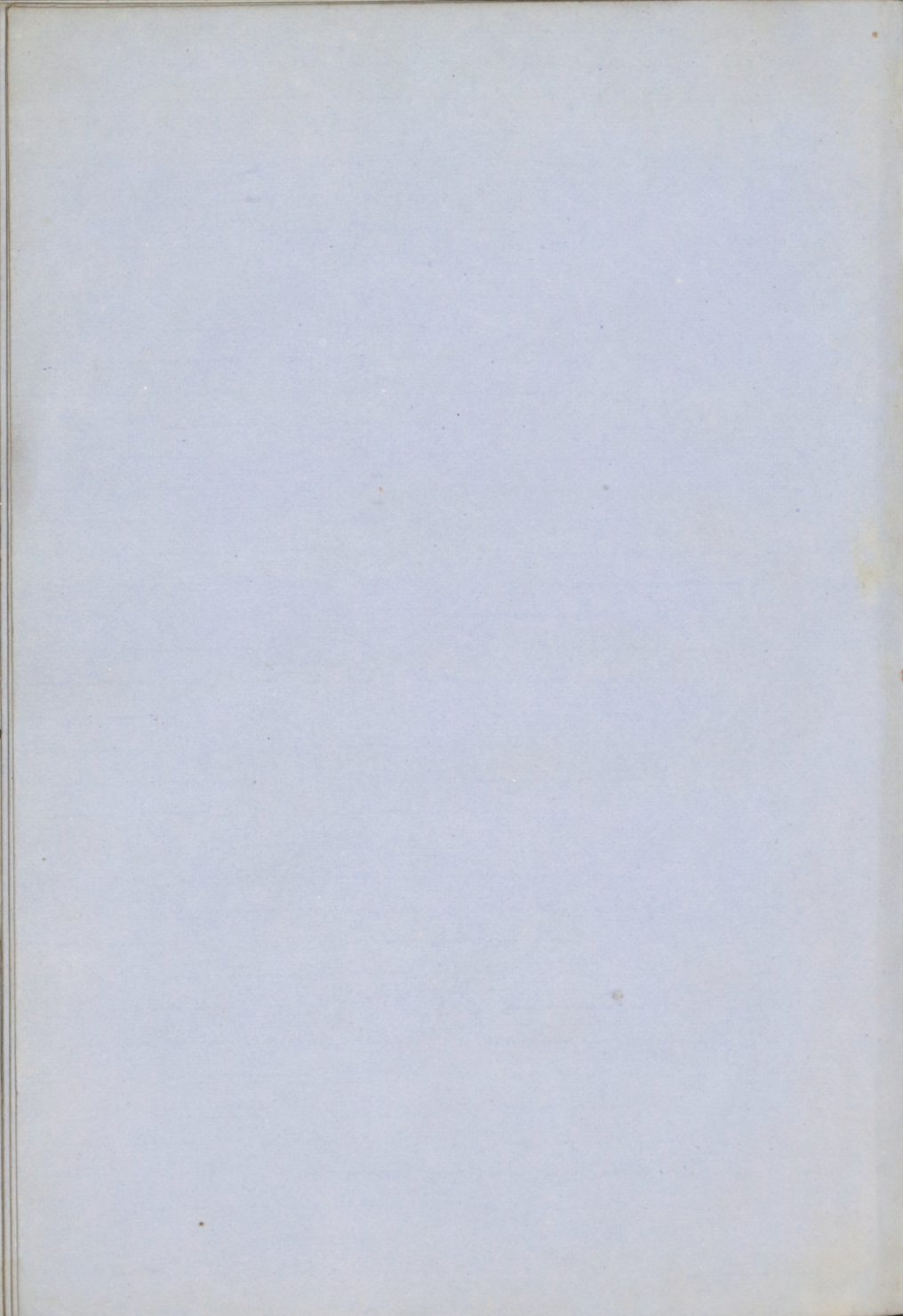
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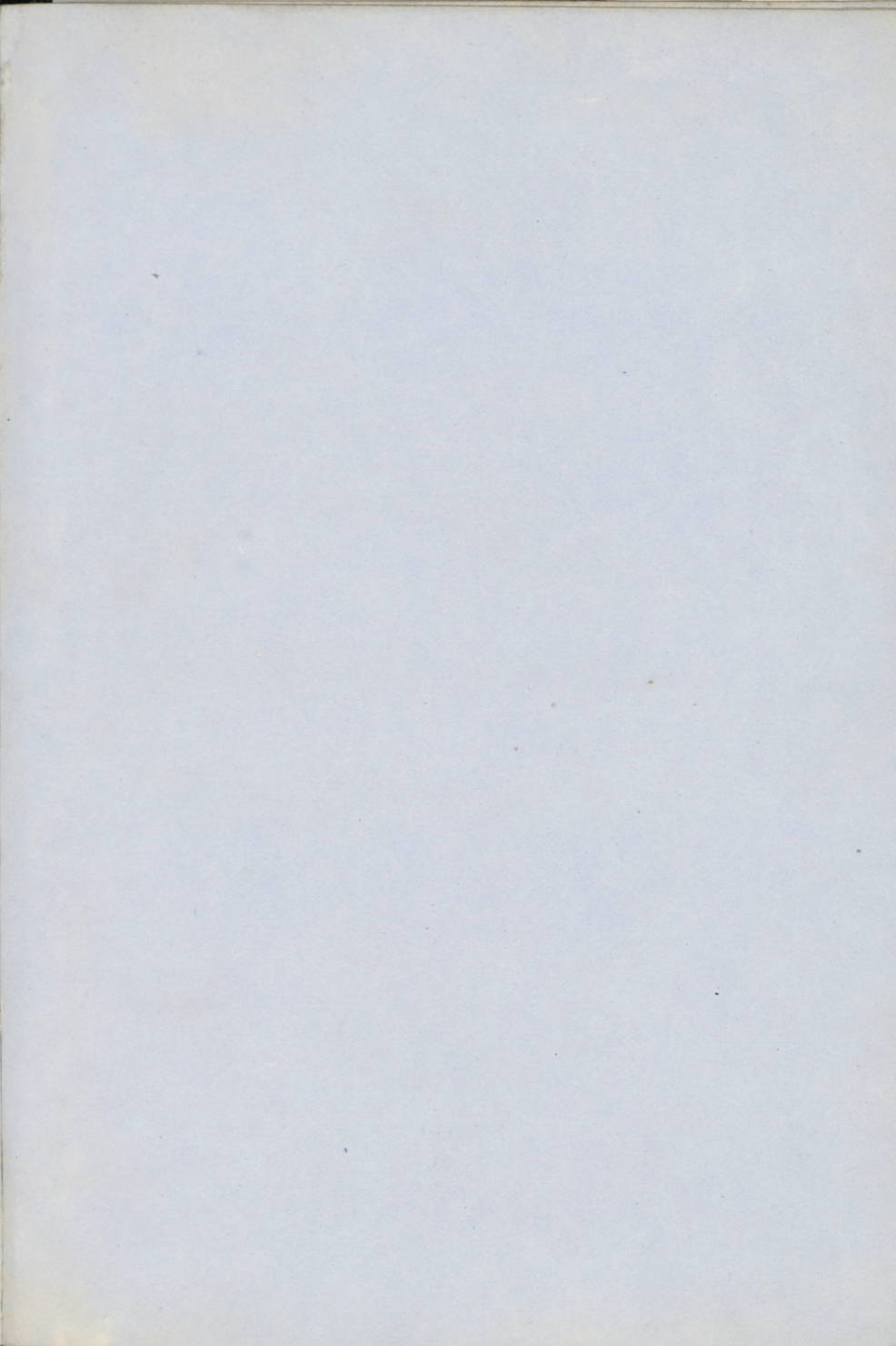
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LLOYD'S REGISTER
OF
BRITISH AND FOREIGN SHIPPING.

RULES AND REGULATIONS.

1. THE operations of the Societies of the two Register Books of Shipping formerly printed for the use of Merchants, Ship-Owners, and Underwriters, having ceased in the year 1834, this Society was then established for the purpose of obtaining a faithful and accurate Classification of the Mercantile Shipping of the United Kingdom, and of the Foreign Vessels trading thereto, and for the government of which the following Rules and Regulations have been from time to time adopted.

2. A Register Book to be printed annually for the use of Subscribers.

3. Each person subscribing the sum of Three Guineas per annum (or such other sum as the General Committee may fix), to be considered a Member of the Society, and entitled *for his own use* to one copy of the Register Book.

4. The subscription of Public Companies, or Public Establishments (not being engaged in Marine Insurance), to be Ten Guineas per Annum.

5. The subscription of Marine Insurance Companies to be regulated by the Committee on special application, in each case, but not to be less than Ten Guineas per Annum.

6. The Register Book to be periodically posted throughout the year.

7. For the convenience of Subscribers not resident in London, a Supplement, containing the additions to, and corrections made in, the Register Book, to be printed fortnightly in such convenient form, as to admit of its transmission by Post, so that such parties may be furnished, from time to time, with the latest and most complete information.

8. The superintendence of the affairs of the Society to be under the direction of a Committee in London, of twenty-four members, consisting of an equal proportion of Merchants, Ship-Owners, and Underwriters.

The Chairman for managing the affairs of Lloyd's, and the Chairman of the General Ship-Owners' Society, and also the Chairman and Deputy Chairman of the Liverpool Committee, and the Chairman of the Rotation Committees for the time being, to be, *ex officio*, Members of the Committee.

9. Six of the Members, namely, two of each of the constituent parts of the Committee, to go out annually by rotation, but to be eligible to be re-elected.

10. The vacancies so arising to be filled up by the election of two Underwriters and one Merchant by the Committee for managing the affairs of Lloyd's, and two Ship-Owners and one Merchant by the Committee of the General Ship-Owners' Society.

11. The Committee to appoint from their own body, annually, a Chairman and Deputy Chairman, and also a Chairman for a Sub-Committee of Classification.

12. The Committee to appoint a Sub-Committee of Classification, to be so regulated that each Member of the General Committee may, in rotation, take his turn of duty therein throughout the year.

13. The Secretary, Clerks, and Servants of the Society, and the Surveyors, to be appointed by, and be under the direction of the General Committee.

14. Special meetings to be convened by order of the Chairman, or Deputy Chairman, or on the requisition of any three members.

15. All elections and appointments to be made by ballot.

16. No Member of the Committee to be permitted to be present on the decision of the classification of any ship of which he is the owner, or wherein he is directly or indirectly interested.

17. The Committee to be empowered to make such By-laws for their own government and proceedings as they may deem requisite, not being inconsistent with the original Rules and Regulations under which the Society was established; but no new Rule or By-law to be introduced, nor any Rule or By-law altered, without special notice being given for that purpose at the meeting of the Committee next preceding that at which such Motion is intended to be made; such notice to be inserted in the summons convening the meeting. No new Rule, or alteration in any existing Rule, materially affecting the classification of ships, to take effect until the expiration of six months from the time it shall have been determined upon.

18. All Reports of survey to be made in writing by the Surveyors according to the forms prescribed, and submitted for the consideration of the General Committee, or of the Sub-Committee of Classification; but the character assigned by the latter to be subject to confirmation by the General Committee.

19. The reports of the Surveyors, and all documents and proceedings relating to the classification of ships, to be carefully preserved, and parties proving themselves to be interested therein, to have access to the same under the direction of the Chairman or Deputy Chairman.

Copies of the original reports (if the ships be already classed, but not otherwise), so far as relates to the dimensions, scantlings, fastenings, and materials, in cases where the correctness of the reports in these particulars is certified by the builders, are granted on application.

20. Foreign ships, and ships built in the British possessions abroad where there is not a Surveyor (*See also Section 51*), to be surveyed on their arrival at a port in the United Kingdom; but a due regard is to be had to the circumstance of their having been exempted from the supervision while building to which all British ships are subjected, and the character to be assigned to them is to be regulated according to their intrinsic quality, and from the best information the Committee can obtain.

21. In every case in which the Character assigned to a ship may be proposed, on survey, to be reduced, notice is to be given in writing to the Owner, Master, or Agent, with an intimation that if the reduction be objected to, the Committee will be ready to direct a special survey, on the Owner, Master, or Agent, agreeing to pay the expenses attending the same, provided on the said survey there shall appear sufficient ground for the proposed reduction.

22. When the Surveyors consider Repairs to be requisite, they are respectfully to communicate the same in writing to the Owner, Master, or Agent, and if such repairs be not entered upon within a reasonable time, a corresponding report is to be made to the Committee for their decision thereon.

23. Parties considering the repairs suggested by the Surveyor to be unnecessary or unreasonable, may appeal to the Committee, who will direct a special survey to be held; but should the opinion of the Surveyor be confirmed by the Committee, then the expense of such special survey is to be paid by the party appealing.

24. The Surveyors to the Society not to be permitted (without the especial sanction of the Committee), to receive any Fee, gratuity, or reward whatsoever for their own use or benefit, for any service performed by them in their capacity of Surveyors to this Society, on pain of immediate dismissal.

25. The Surveyors will be directed to attend on Special Surveys of ships while building or under damage or repair, when required by Merchants, Ship-Owners, or Underwriters; the charge for which is to be regulated according to the nature and extent of the service performed. In all cases, the application for the assistance of the Surveyors must be made in writing addressed to the Secretary.

FUNDS.

26. The Funds to be under the authority and control of the Committee, and a statement of the Receipts and Expenditure to be annually printed for the information of the subscribers.

27. The following Fees to be charged to the Owners of ships prior to their vessels being classed and registered in the book.

I.

For Entering and Classing Ships, and for Entering and Classing Ships surveyed for Continuation, or the Character A in red, or repaired for Restoration.

For each Ship ...	...	under	100 Tons ...	£1 0 0
Ditto ...	of 100 Tons and under	200 ...	2 0 0	
Ditto ...	200 —	300 ...	3 0 0	
Ditto ...	300 —	400 ...	4 0 0	
Ditto ...	400 and upwards ...	... ..	5 0 0	

II.

For Registering Repairs.

For each Ship ...	...	under	300 Tons ...	£0 10 0
Ditto ...	of 300 Tons and under	500 ...	1 0 0	
Ditto ...	500 —	1000 ...	2 0 0	
Ditto ...	1000 and upwards ...	... ..	3 0 0	

For Re-classing Ships (except when repaired) the Characters of which have been expunged, or change of Owners.

For each Ship ...	...	under	200 Tons	£0 10 0
Ditto ...	...	200 — and above	1 0 0	

Special Surveys.

28. For ships built under the special superintendence of the Surveyors (to entitle them to the distinctive mark ∇) 1s. per ton.

For Surveys for damage, or for other Surveys, held at the request of the Owners, and for the Survey of Ships for Restoration, Continuation, or the character A in red, a charge (in addition to the Fee for entry) will be made, according to the nature and extent of the service performed. In cases where the caulking of ships is superintended and tested by the Surveyors, a special charge will be made, according to the tonnage of the ship. All repairs which may be required on the surveys above referred to, must be performed under the superintendence of the Society's Surveyors.

29. Certificates of Character, of the Form No. 7, signed by the Chairman of the General Committee, or by the Chairman of the Sub-Committee of Classification, and countersigned by the Secretary, will be granted on application; the charge for which will be as follows:—

For Ships under 200 Tons	2s. 6d. each.
200 — and above	5s. each.

Copies of original reports, as per Section 19 ... 17. 1s. each.

30. Rules, each copy 2s. 6d.

CHARACTERS.

31. The Characters to be assigned to ships to be, as nearly as possible, a correct indication of their real and intrinsic qualities,* and to be in all cases fixed (not by the Surveyors, but) by the Committee, after due consideration of the Reports of the Surveyors and such other documents as may be submitted to them, and will be distinguished as follows:—

SHIPS A

To consist of new ships, or ships Continued, or Restored. *Vide Sections 34, 54, 55, 57.*

SHIPS A, in Red,

To consist of ships which have passed the period assigned on the original survey, or Continuation, or Restoration; and also of ships not having had an original character, and which are found on survey of superior description, fit for the conveyance of dry and perishable goods, *to and from all parts of the world.* *Vide Section 60.*

SHIPS Æ

To consist of ships which are found on survey fit for the safe conveyance of dry and perishable goods on *shorter voyages.* *Vide Section 61.*

SHIPS E

Will comprise ships which shall be found on Survey fit for the conveyance of cargoes not in their nature subject to sea damage *on any voyage.* *Vide Section 64.*

SHIPS I

To consist of ships fit to carry cargoes not liable to sea damage *on shorter voyages.* *Vide Section 66.*

32. In all cases in which the application of the rules must necessarily be regulated by the ship's admeasurement, the *gross register tonnage* is to be adopted.

RULES FOR CLASSIFICATION.

SHIPS A

33. Will consist of new ships and those which have not passed a prescribed age, provided they are kept in a state of complete repair and efficiency. The character A will not, however, be granted to any vessel, unless satisfactory evidence of the date of build and place where built is produced.

* Ships which are not built in accordance with the principles of the Society's Rules will be marked in the Register Book thus, "*Expt. (B.S.)*," denoting that they are built experimentally, and are classed subject to being surveyed biennially.

34. The number of years to be assigned for this Character to be determined with reference to the original construction and quality of the vessels, the materials employed, and the mode of building; and their continuance for the time so assigned to depend upon its being shown by occasional surveys (annually if practicable) that their efficiency is duly maintained. In all cases the windlass is to be examined by unhooking and stripping the wood linings within a period not exceeding one-half of that originally assigned. The characters of ships A will be struck out of the Register, unless re-surveyed within a period of four years from the *date of last survey*,—or, in the case of ships exceeding the eight years' grade, within *one-half of the time* assigned. After the expiration of the periods prescribed, ships will be permitted to Continue in the character A, or may be Restored thereto, for a further limited period, on complying with the conditions hereinafter prescribed in Sections 54, 55, and 57.

35. New ships are to be surveyed while building, by the Surveyors to this Society, in the following three stages of their progress, or they will be liable to lose one year of the period to which they might otherwise be entitled. (*See Section 53.*) Ships intended to be built under *special survey*, must be placed under the Surveyor's inspection from their commencement, so that all parts of the timbers, deadwood, keel, stem, &c., may be examined.

First.—When the Frame is completed, timbers dubbed fair inside and outside ready to receive planking, and before any planking is wrought.

Second.—When the Beams are put in, but before the Decks are laid, and with at least two strakes of the plank of the ceiling between the lower deck and the bilge unwrought, to admit of an examination of the inner surface of the plank of the bottom.

Third.—When completed, and before the plank be painted or payed.

All Ships for which a higher character than Ten Years' A may be claimed, must undergo a Survey by a Surveyor who is an exclusive Officer of the Society, twice at least, while building; namely, at the first and at the second stages of their progress as above prescribed. Due notice must be given by the Builder or Owner of their being ready for this additional survey.

36. A full statement, agreeably to Form No. 4, of the dimensions, scantlings, &c. of all new ships, verified by the builder, is to be transmitted by the Surveyor, and to be kept as a record in the office of the Society.

RULES TO BE OBSERVED IN BUILDING SHIPS.

37. The whole of the Timber to be of good quality, of the descriptions

shown in Table A as applicable to the several terms of years for which ships so constructed may respectively be appointed to remain on the character A. The stem, stern post, beams, transoms, apron, knightheads, hawse timbers, and keelson of ships claiming to stand *twelve years*, to be entirely free from all defects; the frame to be well squared and free from sap; each set of timbers to be frame-bolted together throughout their entire lengths. The butts of the timbers to be close, and not to be less in thickness than one-third of the entire moulding at that place, and to be well chocked with a butt at each end of the chock. In all cases in which the heads and heels of the timbers shall be *square*, in vessels intended for the twelve years' grade, a dowel (to be in diameter from one-fourth to one-third of the moulding of the timber) must be introduced into the ends of such timbers in order to connect them together. In the construction of top-gallant forecastles, and poops, the timbers must be of the same materials as are required by the Rules (Table A) for the top-timbers of the frames of ships according to the several terms of years appointed for such ships to remain on the character A, all the said timbers to extend to the planksheer. All the outside planking of top-gallant forecastle, and the sheerstrakes, planksheers, and spirketting of top-gallant forecastles and poops must be of the materials required by the Rules (Table A) for the topsides of the said ship; and the shelf and clamps of poops and top-gallant forecastle may be of the same quality as those allowed in Table A for the shelf and clamp of the upper deck. All the beams of top-gallant forecastle, and the mast beams, breast beams, and transom beams of poop, to be of the materials required by the Rules (Table A) for the beams of the said ships; the remainder of the beams and the water-way of the poop, and the remainder of the planking of poop and top-gallant forecastle may be of cedar, mahogany, Baltic or American red pine, pitch pine, larch, hackmatack, tamarac, or cowdie, and rock elm for such remainder of beams only, and yellow pine or American white spruce in ships below the seven years' grade.

This Rule does not apply to raised quarter decks, the materials of which are required to be of the same quality as those named in the Table for the main body of the ship.

38. The Scantlings of the timbers, keelson and keel, thickness of planking, &c. are not to be less than those shown in Table B., following page 32.

In the inside and outside planking, waterways, planksheers, and flat of deck of full poops* and top-gallant forecastles, a reduction of *one-fourth* from the thickness required by the Table B for such planks in the range of

* Parties desirous of making any alteration in the construction of *Poops*, with a view to diminishing the weight (but preserving the requisite strength), may submit their plans for the Committee's consideration and approval.

the upper deck in ships with two decks, will be allowed; and in the siding and moulding of the top timbers and beams of full poops and top-gallant forecastles, a reduction of *one-fifth* will be allowed.

In the inside and outside planking, waterways, planksheers, and flat of deck of raised quarter decks, a reduction of *one-fifth* from the thickness required by the Table B for such parts in the range of the upper deck in ships with two decks, will be allowed.

39. The intermediate dimensions for the scantling of timbers between the floor heads and the gunwale to be regulated in proportion to the distance from the two points. Should the timber and space be increased, the siding of the timbers to be increased in proportion. Whenever ships are built with double floors, thick strakes (see Table B) must be worked inside, to extend from the lower part of the short floor-head chocks to the upper part of the long floor-head chocks, and be well bolted through and clenched, *with one bolt at the head of each long and short arm of floors, and at the heel of each first and second foothook which come upon them, from the foremast extending a distance aft equal to three-fifths of the length of the ship; in such cases, the timber strakes need not be through bolted.*

SHIPS OF GREAT LENGTH IN PROPORTION TO THEIR BREADTH OR DEPTH.

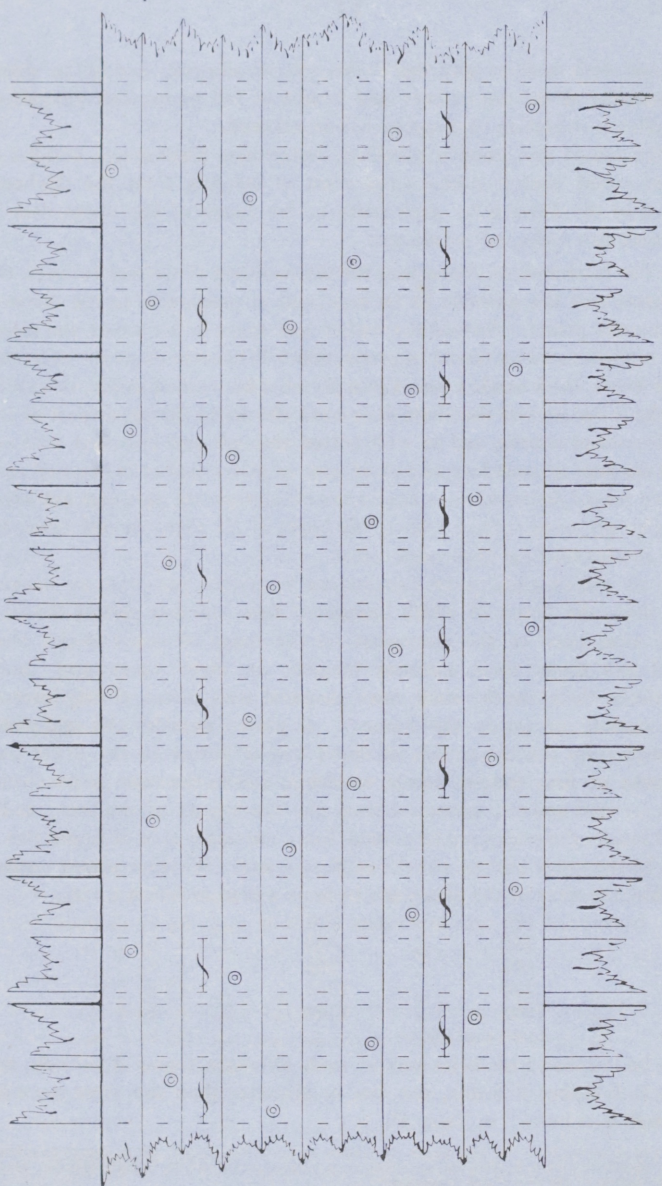
All ships, the length of which (measured from the fore part of the stem to the after part of the stern-post on the range of upper deck), shall exceed five times their extreme breadth, or eight times and under nine times their depth, shall have diagonal iron plates closely inserted either outside or inside the frame.* If placed outside, the said plates to extend from the upper side of upper tier of beams to the lower part of chocks at first futtock heads amidship, and to the same perpendicular height forward and aft, measured from the lower part of the keel; and if placed inside, the plates are to extend from the upper side of upper tier of beams to the lower part of chocks at *floor heads*. Whether placed outside or inside, the sizes of the plates not to be less than as follows, viz. :—

In ships of 100 tons and under 200 tons	...	3½	by	$\frac{7}{16}$	in.
" 200 " 400	"	4	by	$\frac{1}{2}$	"
" 400 " 700	"	4	by	$\frac{5}{8}$	"
" 700 " 1000	"	4½	by	$\frac{3}{4}$	"
" 1000 " 1500	"	5	by	$\frac{7}{8}$	"
" 1500 and above	... ..	5½	by	$\frac{7}{8}$	"

and to be fastened with bolts, one at each alternate timber if outside, and one at each timber if inside, not less in diameter than the sizes given for "through butt bolts" in Table D.

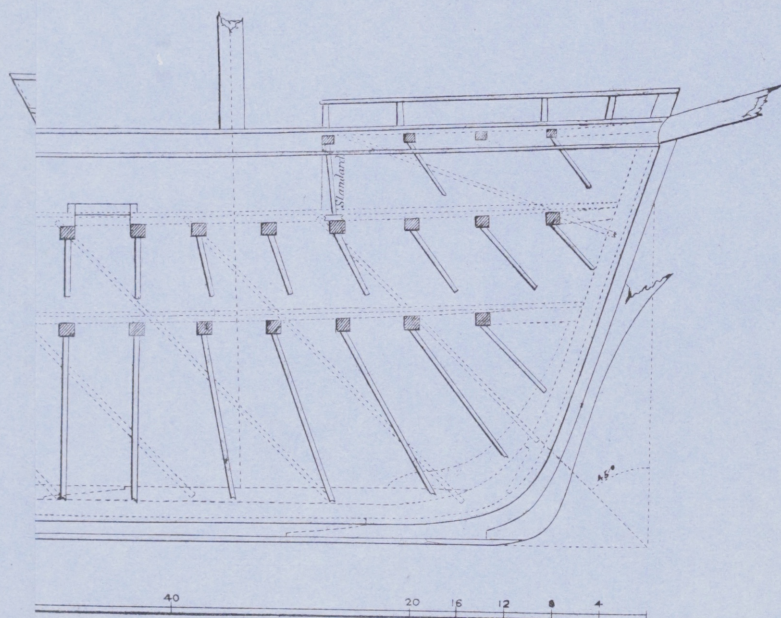
* Parties objecting to fit the iron plates on frames as prescribed above, are at liberty to submit, for the Committee's consideration and approval, such compensation as will, in their opinion, render the introduction of the iron plates unnecessary.

SKETCH SHOWING THE ARRANGEMENT OF THROUGH BOLTS IN THICK STRAKES OVER DOUBLE FLOORS. SECTION 39.



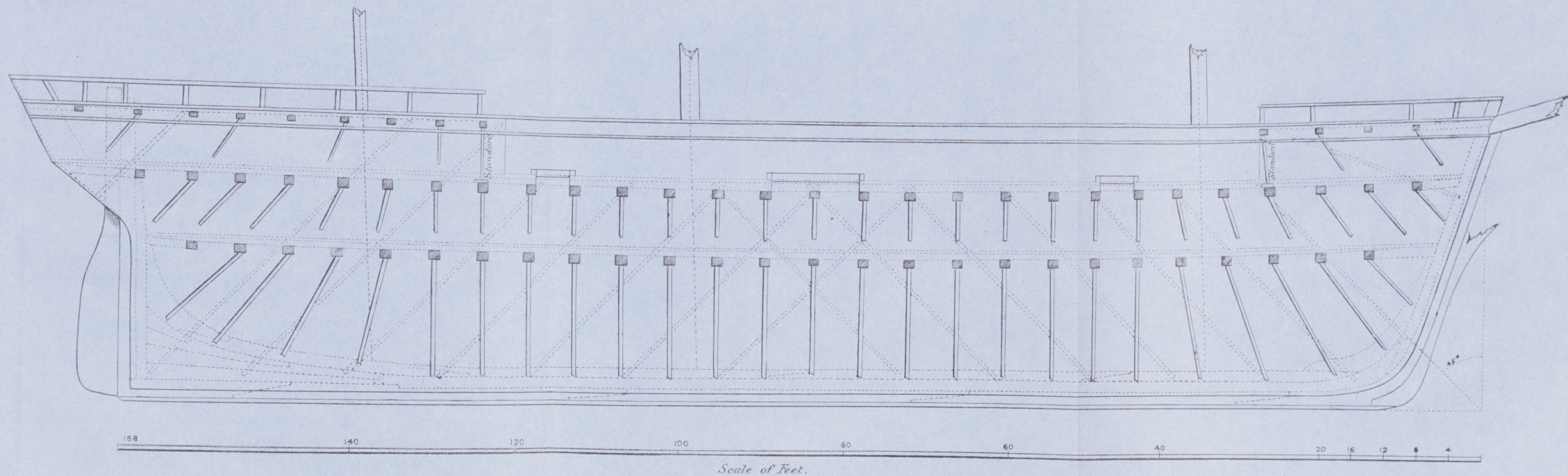
SHIPPING.

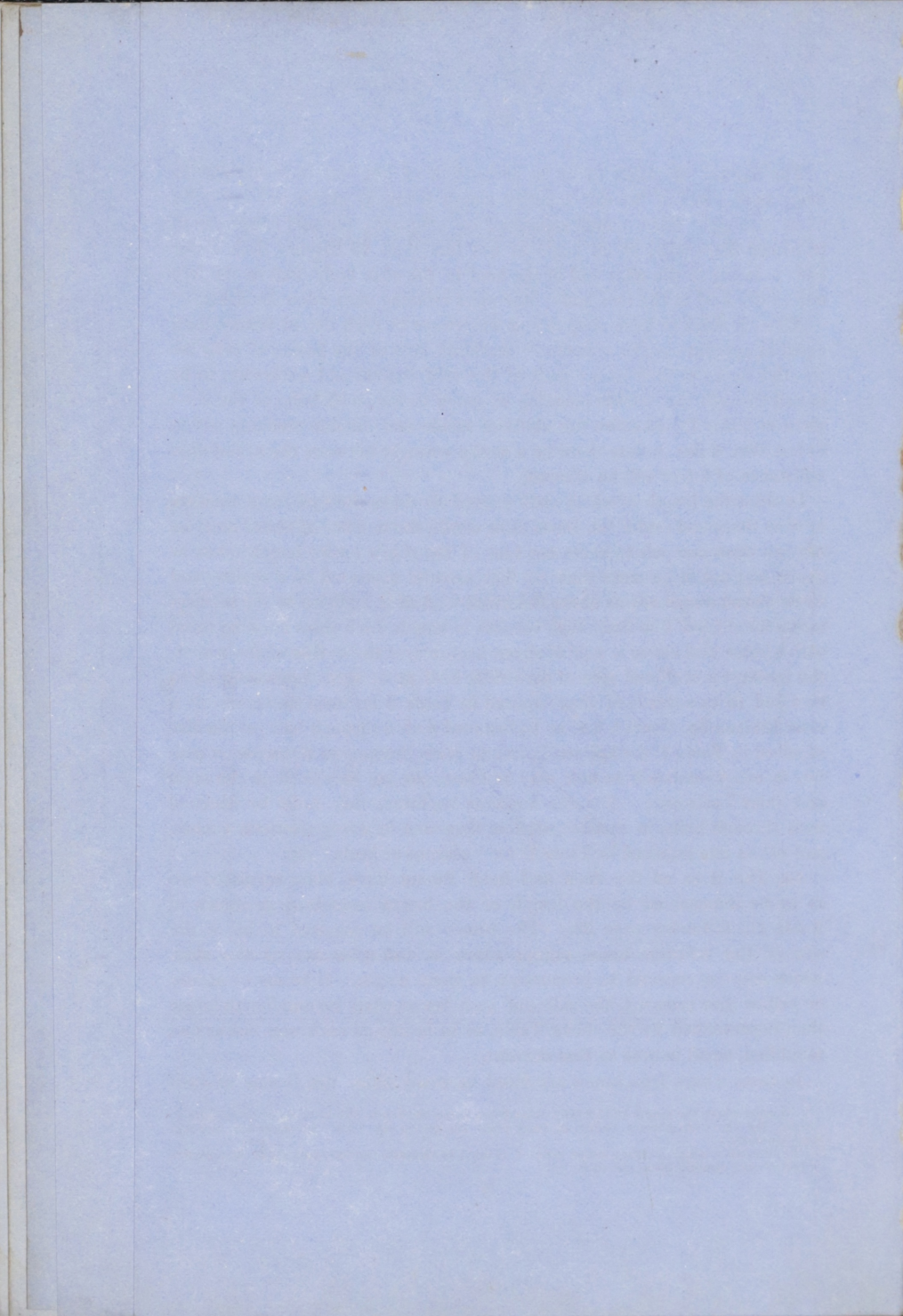
Iron Knees and Riders.



LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

A plan shewing the direction of the Iron Plates on Frames, and Iron Knees and Riders, prescribed in the Rules, Sections 39 and 62.





The number of plates to be in proportion of not less than one pair to every twelve feet of the ship's entire length taken as above, but not to be more than eight feet asunder measured on a square; the said plates are to be placed diagonally, at an angle of not less than 45 degrees, their lower ends pointing to the after end of the keel in the after body, and to the fore end of the keel in the fore body, four pairs crossing each other amidship.

All such ships to have shelves and waterways to each tier of beams, each equal in contents to the transverse sectional area of the beams of their respective decks at their ends; each of the said shelves and waterways to be bolted through the outside planking at every timber, with bolts of the sizes given in Table D; likewise the shifts of inside and outside planking not to be less than 6 feet, unless there be a strake wrought between them, and then a distance of 5 feet will be allowed.

In ships the length of which shall exceed *six* times their extreme breadth, or nine times and under ten times their depth, the number of plates must be not less than one pair to every *ten* feet of the ship's entire length taken as above, but not to be more than six feet asunder measured on a square, and to be placed diagonally as above described.* And in addition to the requirements for ships of five times their breadth in length, such ships must be fitted with a rider keelson or a pair of sister keelsons, at the option of the owner, the transverse sectional area of such rider keelson or sister keelsons each to be equal to two-thirds of that required in Table B for main keelsons. If a rider keelson be adopted, it is to be fastened with a through bolt (of the size required in Table D for keelson bolts), in every frame; or if the owner prefers it, every intermediate bolt may be short, passing only through the main and rider keelsons.† If sister keelsons be fitted, they must be fastened with through bolts, in number not less than one in every alternate timber, and of the size required in Table D for "scarphs of keels," &c.

40. The sizes of the deck and hold Beams have been regulated so as to be determined by the length of the beams *amidships*, as shewn in Table C., following page 32. The beams will be required to be of the size of the midship beam, except those at the *after end* of the ship, which may be reduced in proportion to their length. If beams of spruce or yellow pine are used, the siding of such beams shall be one-fourth larger than is prescribed by the above Table, or be increased each way, siding and moulding, equal in area to that amount.

In cases where Iron beams are fitted in wood Ships the Beams of each

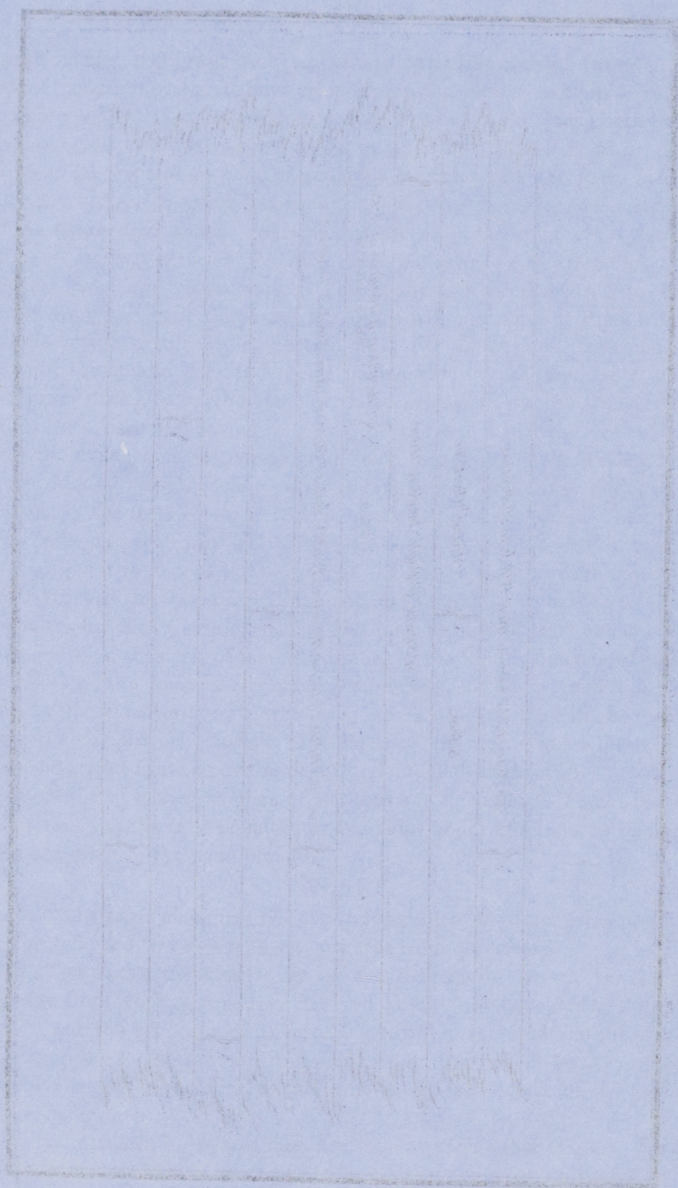
* In cases where the length of the Ship exceeds ten times the depth, the builders or owners are to submit, for the Committee's approval, their plans for giving the vessel the necessary strength longitudinally.

† In all cases in which a rider keelson is fitted, it must be fastened as prescribed above, irrespective of the relative dimensions of the ship.

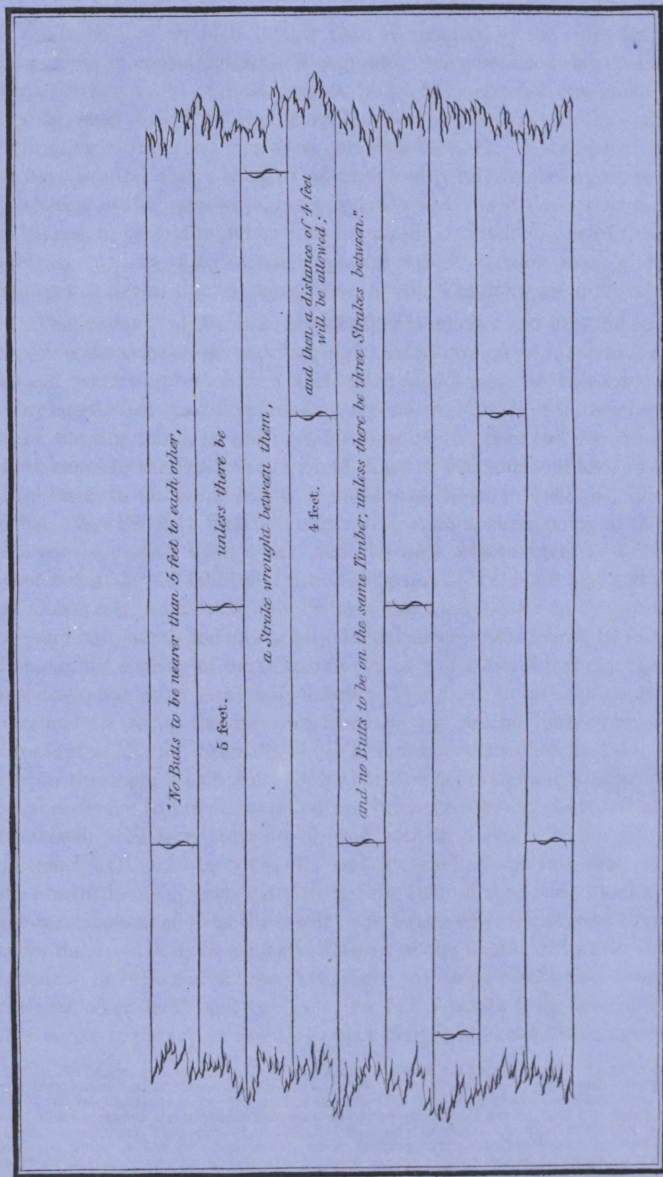
deck are to be $\frac{1}{8}$ of an inch thicker than is required by the rules for Ships built of Iron, in consideration of the greater space between them; and the depth of lower deck or hold Beams is to be increased one inch. The spaces between beams of the several decks not to exceed the spaces at present allowed for wood Ships, as per Rule Sec. 41. Each tier of beams must have stringer plates rivetted on their ends, and tie-plates fore and aft, on each side of the hatchways, in accordance with the Rules for Iron Ships, Sec. 15, and to be of the dimensions required in Table G., or of strength equal thereto. In other respects parties are to submit their plans for attaching the Beams to the Ship's sides, for the Committee's approval.

41. The Beams of all Decks to be sufficient in number and size, and securely fastened to the sides either with lodging knees of iron or wood, or with a shelf piece and waterway,* or with a shelf-piece and knees, or with some other security equal thereto, so as sufficiently to connect the ends of the beams to the sides of the ship: and, in addition, all ships of 150 tons and above to have vertical knees to the DECK beams; and those of 200 tons and above to have vertical knees to the HOLD beams, in number as shewn in Table E., following page 32. Vessels of 13 feet, and under 15 feet hold, the spacing of the hold beams not to exceed 8 feet apart, and the deck beams 4 feet:—Vessels of 15 feet and under 18 feet hold, the spacing not to exceed 8 feet and 4 feet apart alternately, or in that proportion; the deck beams to be placed one over every hold beam, and one in all double spaces:—Vessels of 18 feet hold and above, the spacing of the beams not to exceed 4 feet 6 inches; the deck beams to be one over every hold beam. The depth in all such cases to be determined by taking the measure from the top of the limber-strake (the thickness of which, for measurement to be taken as prescribed in Table B), to the top of the upper deck beams. Ships having a depth of hold, measured from the limber-strake to the under side of the lower deck beam, above 13 feet but not exceeding 15 feet, must be secured with iron riders of the sizes, and be fastened, as shewn in Table F., and in number not less than one on every fourth floor, on each side, from fore side of fore-mast to aft side of mizen-mast, to extend from the lower deck beams downwards so as to receive not less than two bolts in a substantial part of the floors; or by orlop beams, sufficient in number and properly secured. All ships having two decks (viz. upper and lower deck), and exceeding 24 feet in depth from the top of the limber-strake to the top of the upper deck beams, or having three decks (viz.

* When the transverse sectional area of the shelf-pieces and waterways are each equal in contents to the transverse sectional area of the beams of their respective decks at their ends, as given in Table C, and the beams are either dowelled or dovetailed to their shelf-pieces and waterways, and the shelves and waterways are properly scarphed and through bolted, having also a hanging knee to the lower side of every beam end, then lodging-knees may be dispensed with, except in the mast-rooms. In Ships of 500 tons and under, where lodging knees properly bolted are applied, the ordinary plank clamps may be used, but the through bolting of them cannot be dispensed with.



SKETCH DESCRIPTIVE OF THE REQUIRED SHIFTING OF PLANK.—Section 45. (See also Section 39.)



The Sketch shows the principle on which the Butts should be arranged, so as to avoid Stepping, which is deemed bad Workmanship.

upper, middle, and lower deck) and exceeding 24 feet in depth from the under side of the MIDDLE DECK, to have orlop beams, the number to be in no case less than one half the number of lower deck beams in the space between the foremast and the mizen-mast, *except in the case of flush deck ships, when a depth of 25 feet will be allowed, provided in either case the lower hold does not exceed 15 feet, measured as above from the limber-strakes to the under side of the lower deck beam.* Should a house be constructed on a flush-deck ship for lodging crew or for store room, the same not to extend aft beyond 10 feet from the mizen-mast. The application of this Rule to British North American built Ships and Fir Ships will not exempt them from the full operation of the Rule, Section 62. All dimensions, fastenings, and bolts of the middle deck in Vessels having three decks (viz. upper, middle, and lower deck), to be the same as those prescribed in the Tables for the upper deck of ships having only two decks; and a reduction of one-sixth from the dimensions, fastenings, and bolts, prescribed in the Tables, for the upper deck of vessels having only two (viz. upper and lower deck), will be allowed in the third or upper deck, by some called a spar deck. The middle deck to be a perfect deck laid and caulked. The united lengths of poop and forecastle not to exceed three-fifths of the entire length of the upper deck. All timbers of the frame including poop and forecastle to extend to the extreme height. Every ship exceeding 150 tons to have at least one crutch for the security of the heels of the after-timber of the frame; one pair of pointers in addition to a knee at each end of the wing transom to connect the stern frame with the after-body of the ship; and a transom over the heels of the stern timbers properly kneed. The heels of the cant timbers forward and aft to be stepped into or on the deadwood and bolted through.

42. Shifts of timber in ships of 200 tons and upwards, to be not less than one-seventh of the main breadth; and in ships under 200 tons, to be not less than one-sixth of the main breadth.

PLANK.

43. The outside planking to be of good quality, of the description prescribed in Table A, and to be clear of sap and free from all defects.

44. The inside planking to be of the description shown in Table A, and free from all foxy, druxy, or decayed planks. With regard to the ceiling plank, and the efficiency of its fastening, it will be required that the planking shall be properly shifted and fastened so that there shall be at least either treenails or through bolts, or short bolts, in each plank of the ceiling in every timber.

45. No butts to be nearer than five feet to each other (*see also Sec 39.*), unless there be a strake wrought between them, and then a distance of four

feet will be allowed; and no butts to be on the same timber, unless there be three strakes between, as more particularly shown in the diagram annexed (*see Plate*), but vessels under 200 tons will be exempted from the full operation of this rule; and in ships of larger tonnage a literal compliance with it will be dispensed with in cases wherein it may be satisfactorily proved that the departure from the rule is only partial, being confined to the ends of the ship, or the planking of the topside, and does not injuriously affect the ship's general strength; but such relaxation will not be sanctioned unless an accurate description of the shifting of the plank be transmitted by the Surveyors, to enable the Committee to form a proper judgment on the case.

The thickness of the plank, according to the tonnage of the ship, is not in any instance to be less than is prescribed in Table B, following page 32.

The breadth of the wales in every case is to be regulated as under, viz.:

When the extreme length of the ship, measured from the fore part of the stem to the after part of the stern post, on the range of upper deck, is six times her depth of hold (and under), the wales are to be in breadth 3 in. to every foot of the depth of hold.

* When the extreme length of the ship is eight times her depth of hold, the wales are to be in breadth $3\frac{1}{2}$ in. to every foot of the depth of hold.

When the extreme length of the ship is ten times her depth of hold (and above), the wales are to be in breadth 4 in. to every foot of the depth of hold.

And other intermediate dimensions in these proportions.

FASTENINGS.

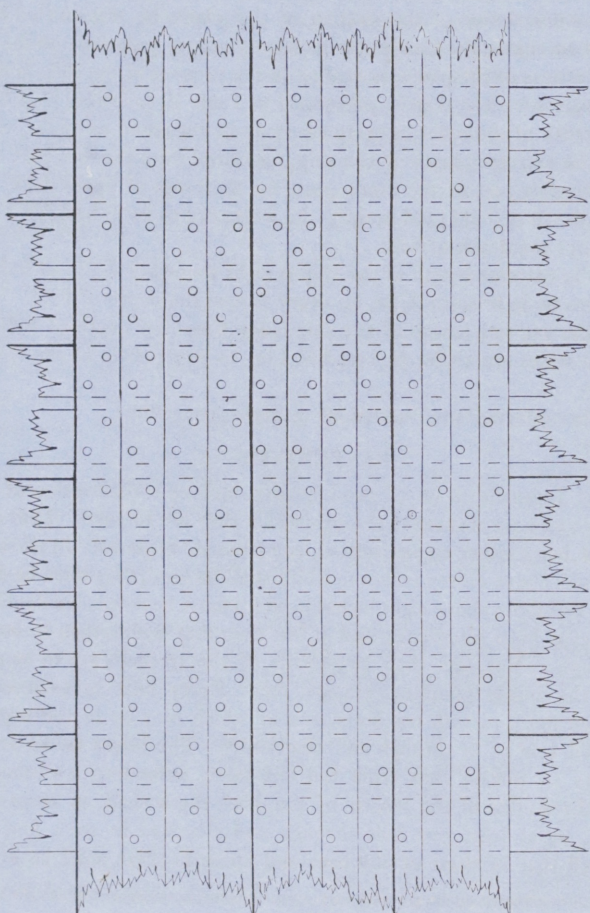
46. The Treenails to be of good quality, and of a description of wood EQUAL TO THE BEST MATERIAL through which they are to pass. They are to be circular, being either engine-turned, compressed, or planed. In all cases in which planks above eleven inches in width shall be used, they must be double fastened; and those above eight inches in width must be treenailed double and single, except bolts intervene; and if less than that width, then to be treenailed single. Not less than two-thirds of the treenails are to be driven through the inside planking, clamps, &c. Every butt in each outside plank to be fastened with *two* bolts, one of which may be in the adjoining timber, and one to be through and clenched.* The bilges to be secured with bolts so placed that from the fore-mast, extending a distance aft equal to three-fifths of the length of the keel, there shall, in ships under 300 tons, be at least one bolt through and clenched in each first foot-hook; and that in ships of 300 tons and upwards there shall be at least

* Where thick garboard strakes are used, they must be bolted horizontally through the keel and each other.

*Single Fastening
in planks 8 inches
wide & under.*

*Double & Single
Fastening in planks
above 8 inches & not
above 12 inches.*

*Double Fastening
in planks above 12 inches.*



two bolts through and clenched for each set of timbers in one or other of the thick bilge strakes. All the bolts of the knees, breasthooks, crutches, riders, transoms, pointers, keelsons, shelf pieces, *waterways*, *heels of timbers against fore and after deadwood*, and of all other material fastenings, are to be driven through and clenched on rings of the same metal as the bolts. The up and down bolts in the knees to beams are not required to be through the deck, but whether clenched upon the beams, or upon the deck, they must be clenched on rings of the same metal as the bolts. The two bolts, the nearest to the crowns of the pintles and braces of the rudder are also to be through and clenched, those through the braces to be in the main piece of stern post. The limber strakes to be bolted down to the floors, and one bolt in every floor, on each side, to be through and clenched.* When the heels of the first foothooks meet at the middle line on the keel under the keelson (either with full moulding or with *buted* chocks) the through bolting of the limber strakes may be dispensed with. Ships in which the flat of upper deck, poop, and forecastle, are fastened with nails or bolts of Copper, Yellow Metal, or Galvanized Iron, and the whole outside planking of which is fastened with treenails and Copper or Yellow Metal Bolts, to the entire exclusion of Iron Bolts and Iron Nails, and in which no Iron Bolts are used in any part of the Vessel, except—the *frame Bolts*, the *short Bolts of inside planking*, and *short bolts in iron plates on frames*, also *short intermediate bolts in rider keelsons*; *up and down Bolts of knees which clinch upon the beams of the upper deck, poop, or forecastle, when the flat of deck is laid over them*; *up and down Bolts of hold or lower deck beams*; *fore and aft Bolts in arms of knees to beams*; *Bolts of the comings, windlass or bowsprit bitts*; *deck fixtures*; *the upper bolt in knee of head, and athwartship Bolts in knee of head afore the stem*; *Bolts or fastenings incidental to the rigging*; or any fastenings above the respective planksheers of poop, waist, and forecastle—will be allowed an additional period of one Year.

And, in addition thereto, a further period of one Year will be allowed to Ships so fastened, in which the outside planking above the floor heads is also fastened entirely with Bolts of Copper or Yellow Metal in lieu of Treenails.

In all such cases of substitution, the number of bolts must be the same as is already prescribed as above for treenails; the proportion of through bolts must be at least *two-thirds*; † and all the through bolts must be of

* Watercourses are to be properly formed at underside of all floors and foothooks at the limbers on each side of middle line, so as to allow water to reach the pumps freely.

† Whenever *metal fastenings* are used in lieu of Treenails, this proportion must be observed.

malleable metal, and clenched on rings (of the same metal) inside. The sizes of the copper or mixed metal bolts must be as under, viz. :—

In ships of 150 tons and under 200 tons	...	...	...	in.
200 ditto 350	...	...	...	in.
350 ditto 500	...	...	...	$\frac{1}{2}$ in.
500 ditto 700	...	...	...	$\frac{3}{4}$ in.
700 ditto 900	...	...	...	$\frac{1}{2}$ in.
900 and above	...	...	...	1 in.

Smaller sizes must not be used.

and the lengths of the short bolts not less than as follows, viz.—

When used in plank of $2\frac{1}{2}$ inches, to be 7 inches long

—	3	8	”
—	4	10	”
—	5	12	”

and so on in proportion for plank of other thicknesses. The sizes of the bolts required in the several parts must not be less than is shown in Table D, following page 32.

47. In every case where the butt and bilge Bolts are not through and clenched, One Year will be deducted from the period which would otherwise be assigned in the classification of the vessel.

48. The Scantlings and dimensions for all sized vessels to be proportionately regulated, agreeably to Table B, following page 32.

49. Ships surveyed while building, in which *all the materials required for a Twelve Years' Ship shall have been used*, and most of the other requisites for that grade fulfilled, but which, from partial deficiencies, may not appear to be in all respects entitled to the full period, although superior to the description of a Ten Years' ship, may be marked in the Book thus, 11 A; thereby denoting that they are to remain on that grade *Eleven Years*, provided they be kept in a state of efficient repair.

50. Ships surveyed while building, in which every *alternate* set of timbers are frame-bolted together throughout their entire lengths, and the scantling and shifts of the timbers, the thickness and shifts of the planks, and size of fastenings may be the same as are required by the rules, and in which the chocks are wrought with a butt at each end, and the description of materials prescribed in the annexed Table A shall also have been used, but in which the frame is not so well squared as is required for Twelve Years' ships, but which shall be *in other respects* equal thereto, shall be marked “10 A;” thereby denoting that they are to remain on that grade for *Ten Years*, provided they be kept in a state of efficient repair.

51. In all other cases, ships surveyed while building, and constructed of the materials of good quality, hereinafter shown in Table A, will be allowed

the several terms of years respectively appointed, provided they be kept in a state of efficient repair. All ships, *not built under Survey*, whether in the United Kingdom or abroad, for which a character may be claimed, must be placed in dry dock or laid on blocks in order that their bottoms may be seen and properly examined; they will also be required to have *their timbers completely exposed for examination, by a plank or listing* AS THE SURVEYOR, (who must be an exclusive officer of the Society), MAY DIRECT, *being taken out, either inside or outside*, all fore and aft, *on both sides, equal to one entire strake*, at the first foothook heads, and another between decks; *and a few treenails must likewise be driven out*, so that the Surveyors, from actual inspection, may be satisfied whether or not they are of the quality and make prescribed by the Rules; and the same being thus ascertained, shall be reported to the Committee, and a character assigned. If the ship be 400 tons and upwards, the Survey must be made by two Surveyors, and their report signed accordingly.

52. Ships built under a substantial and efficient Roof, kept in good repair, *which shall project at each end beyond the length, and on each side beyond the breadth, a quantity equal to half the breadth of the vessel*, shall have one year added to the period prescribed, provided they shall have been surveyed whilst building, and shall have occupied a period of not less than twelve months in their construction, *and in which no plank, except as follows, shall have been worked until the expiration of at least three months after the frame was completed*, viz.:—not more than three strakes of bilge planks, and two strakes of outside plank in the way of each tier of beams, also the clamps inside, so that the beams may be put in their places.

53. Ships built in the United Kingdom;—or in Quebec after 1851;—or St. John, New Brunswick after 1853;—or Miramichi, and Northern Ports of New Brunswick, or in Prince Edward Island after 1855;—and *not surveyed while building* by the Surveyors to this Society, and all ships, the owners or builders of which may have refused or declined to permit them to be surveyed at the several periods prescribed by the Rules, will have One Year deducted from the period which would otherwise have been assigned, in consequence of their not having been submitted to survey during their construction. In no case, however, will a higher grade than 10 A be assigned to ships built in the United Kingdom, which shall not have been surveyed while building.

CONTINUATION OF SHIPS A.

54. If, on the termination of the period of original designation, or if at any subsequent period not exceeding one-third of the number of Years assigned

originally, or on Restoration, an Owner should wish to have his ship remain, or be replaced on the letter A (*vide section 59*), he is to send a written notice thereof to the Secretary, and the Committee shall then direct a Special Survey as follows to be held by not less than *two* competent persons to be appointed by the Committee, one of them to be a Surveyor the exclusive servant of the Society :—

SURVEY.

For the purpose of facilitating such survey, the ship shall be either placed in dry dock or laid on blocks, so that the keel may be examined, and be scraped from the light water-mark upwards, including the plank-sheer and waterways, so as to expose the surface of the plank to view.* The windlass to be unhung, and wood linings stripped. The hold to be cleared and proper stages made. The attention of the Surveyors shall then be particularly directed to the state of the upper or main deck and comings, the upper and lower deck bolts, and the outside planks through which they pass, the planksheers, waterways, and beams, so far as they can be examined; the hawse timbers, knight-heads, breasthooks, and transoms; the floors and keelsons; the keel and rudder; the planking outside, and the treenails passing through from the light water-mark upwards; the ceiling inside, and the frame and inner surface of the outside planking where it may be seen; and the sheer and general form of the ship; *and should any suspicious treenails or bolts appear, the same are to be driven out for inspection.* The Surveyors on these points shall transmit to the Committee a detailed report, accompanied by such observations as may occur to them, from inspection of the ship, or from information of the repairs she may have received. If from the report of such special survey the ship shall appear to be in a sound and efficient state, and to have preserved her original form unaltered, the Committee shall continue such ship on the letter A for such further period as they may think fit, not exceeding, however, one-third of the number of years which had been originally assigned. Ships classed A for four years, will be allowed a Continuation for *Two Years*, provided that, in addition to the above requisitions, the Owners shall agree to a *strake in the Topsides, fore and aft*, on both sides, being

* If the Ship has been sheathed with wood over felt, fastened with copper or mixed metal nails, within a period of five years, and the plank from the light water-mark upwards shall, when so sheathed, have been brightened, and the condition of the bolts, planking, treenails, and caulking ascertained, and favourably reported upon by the surveyors; and provided that the sheathing which covers the binding bolts and raft ports, and a strake of sheathing all fore and aft on each side under the wales be removed, and listings of sheathing cut out at hood ends; and the planking, fastenings, and caulking so exposed, shall prove to be in good condition, then, on application to the Committee, the stripping from the light water-mark upwards may be dispensed with; but whenever the sheathing is removed, the outside planking is to be scraped or dubbed bright, and examined as prescribed by the above rule.

If the ship has been sheathed with metal within a period of two years, and it shall appear to the surveyors that stripping from the light water-mark upwards may be dispensed with, the case will receive due consideration on application to the Committee.

also removed; this, however, will not exempt ships built in the British North American Colonies from the operation of the Rule, Section 63. Ships so Continued shall be distinguished in the Register Book by the number of years for which the character is extended, being inserted separately under the number assigned on the original character, thereby denoting that the ship has been found on survey in such good and efficient order as to entitle her to be Continued for _____ years. The period assigned for Continuation will, upon all occasions, commence from the time the ship may have gone off the letter A, without regard to the date when the survey for this purpose may have been held.

In cases of the repair of ships for Continuation of character under the Rules, section 54, materials of an inferior description (but not below those prescribed for the six years' grade) may be permitted to be used in those parts which must of necessity, under the operation of the Rules, section 56, be *entirely removed* on a repair for Restoration; subject, however, to the ship-owner, in every instance, making a special application to the Committee for their previous sanction.

RESTORATION OF SHIPS TO THE CHARACTER A.

FIRST RULE.

55. If, at any time before the expiration of two-thirds of the number of years *beyond* the period originally assigned, an owner be desirous to have his ship Restored to the A character, such Restoration (on his consenting to the special survey hereinafter described, to be held by two Surveyors, and performing the repairs found requisite) will be granted for a period not exceeding two-thirds of the time originally assigned, the same to be calculated from the date of such repairs.

Requisites for Restoration.

56. All the bolts in the range of each deck to be driven out, and the planks taken out; the upper deck waterways, and planksheers and spirketting, and the strake next the waterways on the lower deck in the midships, to be taken out; * the sheathing to be entirely stripped off the

* In the cases of ships fastened with copper or yellow metal, to the exclusion of iron (in conformity with the Rule, Section 46), where there is no poop or forecastle, if the whole of the plank-sheer and spirketting of upper deck and outside planking equal to one strake all fore and aft be removed in way of fastening bolts to each tier of beams, below the upper deck beams; and where there is a poop and forecastle, then if the whole of the plank-sheer and spirketting of poop, fore-castle, and waist, and a strake of outside planking (in way of fastening bolts to upper deck beams), the entire lengths of the poop and fore-castle, and outside planking equal to one strake all fore and aft be removed in way of fastening bolts to each tier of beams below the upper deck beams; and in all cases a strake of deck next the waterway of each deck, on both sides, be removed, also the throat bolts of all knees, together with the other requisitions relating to the bottom, ceiling, &c., the further removal of fastenings, bolts, and planks, may, under the sanction of the Committee, be dispensed with, provided their condition be carefully ascertained and favourably reported upon by the Surveyors.

The above relaxations, so far as they relate to the removal of plank and fastenings in the range of the lower deck, will be extended to all ships in which all the lower deck fastenings are of copper or yellow metal.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

LAPSING OF SHIPS CLASSED A.

It having been found that the regulation contained in the footnote to Sec. 59, whereby Ships launched in the month of July and subsequent months, have an advantage in the lapsing of their A 1 character over ships launched in the first six months of the year, has given rise to considerable dissatisfaction ;—

NOTICE is hereby given, that in pursuance of a Resolution passed this day by the Committee, the regulation in question has been rescinded, and the period originally assigned for Ships to retain the A 1 character will in every case date from the month in which the vessel may be launched, and will expire at the end of the corresponding month in the year at which the period assigned terminates.

This regulation will come into force in respect to Ships launched on and after the 1st July, 1863. Vessels which may have been launched under the existing Rule, will, of course, be allowed the advantage thereof.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

and all the treenails driven out and renewed, such ship may be Restored. But if timber of an inferior description, or second-hand English or African Oak or Teak be used, then for a period not exceeding that for which such materials would have entitled a new ship to stand A according to the tables, subject, in either case, to the ship being at all times thereafter kept in a state of efficient repair.

59. Ships which have been *Restored* shall be entitled to Continuation, subject to the same conditions of survey and examination as are prescribed for ships proposed to be Continued at the expiration of the period first assigned to them (Sec. 54); but in like manner, the term of such extended Continuance shall be limited to a period not exceeding one-third of the number of years for which the ships may respectively have been *Restored*, without any reference whatever to the period originally assigned to them.

At the termination of the several periods assigned to ships for remaining on the character A, they will have the character A struck through, and the word '*lapsed*' inserted against them; and if not surveyed prior to the reprinting of the Register Book, they will appear without any character.* But if during the *last year* of the period assigned to them, the Owners of a ship shall, in consequence of her being about to proceed on a distant foreign voyage, apply to have her surveyed for continuation on the letter A, or for the character A in red, a special survey shall be held conformably to the Rules, sections 54 or 60, as the case may be: and if from the report of such special survey, the ship shall appear to be in all respects in a sound and efficient state, such as is required by those Rules, the Committee shall, from the period at which the ship's character would terminate, continue her on the letter A, or assign to her the character A in red in accordance with the Rules referred to.

SHIPS A, IN RED.

60. Ships that have passed the periods which have or might have been assigned to them for the character A originally, or on Continuation, or for Restoration, and shall be found on survey to be of a superior description, being fit for the safe conveyance of dry and perishable goods to and from all parts of the world, shall be classed A in Red, as the Second description of the First class.

All British North American built Ships and all Fir Ships, also all Ships, the original construction of which, by their timber materials, may not have

* Ships launched on and after the 1st July, 1859, will retain the characters respectively assigned to them until the issuing of the Register Book for the year commencing on the 1st July next ensuing after the periods for which they have been classed shall have expired, provided they undergo the Surveys required by the Rules, and are kept in an efficient state of repair. Thus, ships launched on the 1st July, 1859, or any other day between that date and the 1st July, 1860, and classed A 1 for 10 years, will lapse on the 30th June, 1870. All ships launched previously to the 1st July, 1859, will remain under the Rules in force when they were built.

entitled them to the A character for a longer period than five years, seeking to be classed with the character A in red, must be surveyed in conformity with the *second* clause of the Rule; and, in addition thereto, such Ships of 400 tons and upwards, must be *doubled*, as prescribed by the Rules, Sec. 68,* from the keel to above the first strake of lower deck spirketting.

In all cases in which the owner may claim this character, the ship must undergo a special survey by two surveyors (to be appointed in every instance by the Committee), one of whom shall be an exclusive officer of the Society, and be subject in other respects to a compliance with the undermentioned requisitions of Survey, viz.:—

FIRST SURVEY FOR A, IN RED.

The ship to be either placed in dry dock or laid on blocks, so that the keel may be examined, and be scraped or dubbed bright from the light water-mark upwards, including the planksheers and waterways, so as to expose the surface of the plank to view.† Iron bolts in number not less in any case than six on each side, in each range of the deck fastenings to be driven out in ships of 500 tons and under, and increased in number in proportion to the size of the ship, and one treenail to be driven out in every alternate frame or fourth timber between the upper edge of the wales and planksheers, and one in every alternate frame or fourth timber between the upper edge of the wales and the light water-mark, and such other parts of the bottom as the surveyors may direct, so as to enable a judgment to be formed as to the general state of the frame, treenails, and inside of the planking. The hold to be cleared, and proper stages made both inside and outside. The windlass to be unhung, and the wood linings stripped. The attention of the surveyors shall then be particularly directed to the state of the upper or main deck and comings, the upper and lower deck bolts, and the outside planks through which they pass, the planksheers, waterways, and beams, so far as they can be examined; the hawse timbers, knightheads, breasthooks, and transoms; the floors and keelsons; the keel, rudder, and windlass; the planking outside and inside, and the treenails; and the frame and inner surface of the outside planking,

* Ships classed originally A for 7 years and under, which may be classed A in red, under the second clause of the Rule, and which have been doubled, will be allowed exemption from Special re-survey for a period not exceeding five years, or for the term (if less than five years) for which they were classed originally.

† If the ship has been sheathed with wood over felt, fastened with copper or mixed metal nails, within a period of five years, and the plank from the light water-mark upwards shall, when so sheathed, have been brightened, and the condition of the bolts, planking, treenails, and caulking ascertained, and favourably reported upon by the surveyors; and provided that the sheathing which covers the binding bolts and raft ports, and a strake of sheathing all fore and aft on each side under the wales be removed, and listings of sheathing cut out at hood ends; and the planking, fastenings, and caulking so exposed shall prove to be in good condition, then, on application to the Committee, the stripping from the light water-mark upwards may be dispensed with; but whenever the sheathing is removed, the outside planking is to be scraped or dubbed bright, and examined as prescribed by the above rule.

If the ship has been sheathed with metal within a period of two years, and it shall appear to the surveyors that stripping from the light water-mark upwards may be dispensed with, the case will receive due consideration on application to the Committee.

where they can be seen ; and the sheer and general form of the ship ; the condition of the oakum and caulking also to be ascertained, and the ship to be efficiently repaired with suitable materials as hereinafter stated.

SECOND SURVEY FOR A, IN RED.

(After two-thirds the number of years beyond that assigned originally, or on Restoration, have expired.)

But when a period of two-thirds of the number of years beyond that originally assigned for a vessel's retaining the A character, or if a period of two-thirds the number of years beyond that assigned on Restoration has elapsed, an owner be desirous to have his ship *retain*, or be placed upon, this character, application must be made to the Committee in writing, who will direct the survey to be made by two surveyors, one of whom shall be an exclusive officer of the Society ; and for the purpose of holding such survey, the ship must be placed in dry dock, or laid on blocks upon ways, so that the keel may be examined ; all sheathing to be entirely stripped off the bottom and elsewhere ; all the outside planking from light water-mark upwards, including the planksheers and waterways, to be scraped or dubbed bright ; the timbers of the frame to be exposed to view by the removal of planking equal to one strake fore and aft, on each side, above the wales, either inside or outside at the discretion of the surveyor ; a short plank in each buttock ; a plank or listing of sufficient breadth, on each side, at the discretion of the surveyor, in the ceiling above the floor heads in midships (or, if the ship-owner should prefer it, a plank outside at the same height), and a reasonable number of treenails before and abaft the same, so as to ascertain the state of the lower timbers of the frame ; and in order to ascertain the condition of the beam ends, either a strake of deck next the waterways on each side to be taken out, or an examination be made by boring, at the option of the ship-owner ; iron bolts and treenails to be driven out at the various parts as prescribed above ;* proper stages to be made both inside and outside ; the windlass to be unhung, and the wood linings stripped ; and when in the state above described, the special survey to be held as above directed upon all the parts exposed to view ; the condition of the oakum and caulking to be ascertained ; the ship to be efficiently repaired with suitable materials, as hereinafter stated. And to entitle them to continue this character, such vessels will be required, in addition to the usual annual survey, to undergo a special re-survey, as prescribed above, within a period (from the date of the last special re-survey) not exceeding *two-thirds* of the several terms of years originally assigned to them, or earlier, if, in the judgment of the Surveyors, upon a careful examination of the ship, the same shall appear to them to be necessary.

* In the case of doubled ships, or ships of peculiar construction, special application may be made to the Committee.

In the repair of vessels for the above character, no materials may be used of a description inferior to those allowed in new Ships for the Six Years' grade.

Whenever it shall appear to the Surveyors that a vessel classed A, in Red, shall no longer be in a condition to deserve that character, notice of the proposal to reduce her shall be given in writing to the Owner, Master, or Agent, in the same manner as is prescribed by the rules, section 21, page 7.

N.B. Ships at present classed with the Æ asterisk character will be allowed to retain the same, subject to annual Survey, until the expiration of the period for which they have already been specially surveyed.

SHIPS Æ.

61. Ships that have passed the prescribed age for the A Character, but have not undergone the repairs which would have entitled them to be Continued or Restored; or having been Continued or Restored, and the additional period thus assigned expired, and also such ships as have never had an original character, which shall be found on survey fit for the conveyance of dry and perishable goods on *shorter voyages*, shall be distinguished by the diphthong Æ; and a careful survey will be required to be made *annually*, or on the return of the ship from every foreign voyage, by one of the Surveyors to this Society, who is to state distinctly and separately the actual condition of the *upper deck fastenings, waterways, spirketting, planksheers, topsides, upper deck with its appendages, lower deck fastenings, wales, counter, plank, and treenails outside to the water's edge, rudder, windlass and capstan, beams, breasthooks, transoms, and timbers*; but if not surveyed within twelve months, such ship having been during that time in some port in the United Kingdom, the character will be omitted until such survey be held; or, as the case may be, she will be allowed to pass into the class E.

62. Ships built in the British North-American Colonies, and all ships wherever built, the frames of which are composed of *Fir*, of 400 tons and above, shall, in order to entitle them to be classed in the Register Book of the Society, be secured in their bilges by the application of iron knee riders, or hanging knees and riders to cover the joints of the floor and foothook heads, to extend from the height of the hold beams to the floors so as to receive not less than two bolts in a substantial part of the floors; the number of iron knees and riders to be not less than one of each to every hold or lower deck beam on each side. The knees to be connected with the riders or not, at the option or convenience of the owners; but if not so connected, the side arms of the knees are to be of the length and to be fastened as pre-

scribed in Table F. The number of knees to each deck, and of riders, also their dimensions, and number of bolts, are fully explained in Table F. All ships built in the Colonies will be considered as "iron fastened" in their centre lines, unless it shall be satisfactorily shown to the contrary, either by the exposure of some of the bolts, or by a certificate to be produced from the builders.

On and after the 1st January, 1858, ships which proceed to sea *without being fastened with the iron knees and riders prescribed by the rules, will have one year deducted from the period to which they would otherwise be entitled to be classed in the Register Book.*

Ships built in the British North American Colonies, and all ships the frames of which are composed of Fir, of 600 tons and upwards, *and all ships (wherever built) the length of which (measured from the fore part of the stem to the after part of the stern-post on the range of upper deck), shall exceed five times their extreme breadth, or eight times and under nine times their depth, shall have diagonal iron plates closely inserted either outside or inside the frame. If placed outside, the said plates to extend from the upper side of upper tier of beams to the lower part of chocks at first foothook heads amidship, and to the same perpendicular height forward and aft, measured from the lower part of the keel; and if placed inside, the plates are to extend from the upper side of upper tier of beams to the lower part of chocks at floor heads. Whether placed outside or inside, the sizes of the plates not to be less than as follows, viz. :—*

In ships of 100 tons and under 200 tons	...	$3\frac{1}{2}$	by	$\frac{7}{16}$	inch.
" 200 " 400	"	4	by	$\frac{1}{2}$	"
" 400 " 700	"	4	by	$\frac{5}{8}$	"
" 700 " 1,000	"	$4\frac{1}{2}$	by	$\frac{3}{4}$	"
" 1,000 " 1,500	"	5	by	$\frac{3}{4}$	"
" 1,500 and above	.....	$5\frac{1}{2}$	by	$\frac{7}{8}$	"

and to be fastened with bolts, one at each alternate timber if outside, and one at each timber if inside, not less in diameter than the sizes given for "through butt bolts" in Table D.

The number of plates to be in proportion of not less than one pair to every 12 feet of the ship's entire length taken as above, but not to be more than eight feet asunder measured on a square; the said plates are to be placed diagonally, at an angle of not less than 45 degrees, their lower ends pointing to the after end of the keel in the after body, and to the fore end of the keel in the forebody, four pairs crossing each other amidship.

All such ships to have shelves and waterways to each tier of beams, each equal in contents to the transverse sectional area of the beams of their respective decks at their ends; each of the said shelves and waterways to be

bolted through the outside planking at every timber, with bolts of the sizes given in Table D; likewise the shifts of inside and outside planking not to be less than 6 feet, unless there be a strake wrought between them, and then a distance of 5 feet will be allowed.

In ships the length of which shall exceed *six* times their extreme breadth, or nine times and under ten times their depth, the number of plates must be not less than one pair to every *ten* feet of the ship's entire length taken as above, but not to be more than six feet asunder measured on a square, and to be placed diagonally as above described.* And in addition to the requirements for ships of five times their breadth in length, such ships must be fitted with a rider keelson, or a pair of sister keelsons, at the option of the owner,—the transverse sectional area of such rider keelson or sister keelsons each to be equal to two-thirds of that required in Table B for main keelsons.—If a rider keelson be adopted, it is to be fastened with a through bolt (of the size required in Table D for keelson bolts) in every frame; or if the owner prefers it, every intermediate bolt may be short, passing through the main and rider keelsons.† If sister keelsons be fitted, they must be fastened with through bolts, in number not less than one in every alternate timber, and of the size required in Table D for “scarphs of keels,” &c.

63. All British North-American built ships, which have gone, or may go off the List of Ships of the A character, or which may be of an age exceeding the period for which they might have had claims to be put upon that grade (whether classed or not), shall, as from time to time they come under examination, be subjected to a careful survey, to be made by one of the Surveyors to this Society;—and no further character shall be assigned them unless a survey shall be held as follows; and planking, either inside or outside, at the discretion of the Surveyors, in quantity equal to *one entire strake* fore and aft on both sides, shall be removed; to be taken out in midships immediately above the turn of the bilge, and *at such height* forward and aft as may, in their judgment, best expose the timbers of the frame to view; that a special report of the state of these timbers, and of the general state and condition of the upper deck fastenings, waterways, spirketting, planksheers, topsides, upper deck with its appendages, lower deck fastenings, wales, counter, plank and treenails outside to the water's edge, rudder, windlass and capstan, beams and breasthooks, shall be transmitted by the Surveyors to the Committee; and on the receipt of such report the character shall be assigned. If the *Æ* character be then assigned, it shall be

* In cases where the length of the Ship exceeds ten times the depth, the builders or owners are to submit, for the Committee's approval, their plans for giving the vessel the necessary strength longitudinally.

† In all cases in which a rider keelson is fitted, it must be fastened as prescribed above, irrespective of the relative dimensions of the ship.

continued (subject to an annual survey) for a period not exceeding the number of years originally assigned; at the expiration of which the character will be discontinued, unless a similar survey and examination of the frame be again submitted to.

SHIPS E

64. Will comprise all ships which shall be found on survey fit for the conveyance of cargoes not in their nature subject to sea damage *on any voyage*.

65. Subject to occasional inspection, at least once in every two years, ships will continue in this class so long as their condition shall, in the opinion of the Committee, entitle them thereto.

SHIPS I

66. Will comprise ships which shall be found on survey fit for the conveyance, *on shorter voyages* (not out of Europe), of cargoes in their nature not subject to sea-damage.

67. The Bottom of every ship is to be CAULKED* once in every five years, unless wood-sheathed and felted, and then once in every seven years, except in the case of *Teak-built ships*, upon which a special survey may have been requested, and the Surveyors having ascertained, by the removal of a strake of sheathing fore and aft under the wales, and a strake at the first foothook heads, and by causing listings to be cut out at the wood's ends, that such caulking is not required, the same may then be dispensed with, If any ship shall be stripped within the periods above mentioned, her bottom is to be caulked, *if necessary*.

68. In all cases in which ships may be doubled, doubling of not less than the thicknesses hereinafter mentioned will be required, the same to be properly wrought and fastened as follows: in every instance the doubling is to be at least single fastened either with treenails or with bolts, and a through bolt in every butt. If treenails be used, every treenail must, if practicable, be a through fastening; and if bolts be used, then one-sixth of them from the lower part of the bilge upwards must be through and clenched on the ceiling in addition to the butt bolts. In all cases of doubling, the rudder braces are to be removed.

* In cases where ships have been doubled with doubling of less thickness than is required by, or not fastened in accordance with, the rules, it will not be imperative that such doubling be stripped at the expiration of seven years as required for ordinary sheathing; but if, upon survey, the doubling be found in good condition, the period for its remaining on may be extended, with the sanction of the Committee, to a term not exceeding ten years, provided the doubling below the wales be copper or yellow metal fastened or treenailed.

The throat bolts of iron knees, and the bolts of iron hooks, crutches, and pointers, must be renewed through the doubling.

The thickness of the doubling for the wales and bottom, on ships

Under 400 tons to be not less than	...	2 inches
of 400 „ and under 600 tons	...	2½ „
of 600 „ and above	...	3 „

On the Topsides of ships not exceeding 300 tons, the thickness may be 1½ inches.

If doubling be applied diagonally it will be allowed to be of the following thicknesses, viz. :—

In ships under 500 tons	...	...	1½ inch
„ 500 tons and under 1000 tons	...	...	2 „
„ 1000 tons and upwards	...	...	2½ „

No ship hereafter doubled, shall be entitled to the character A, or A in red, unless at the time of doubling it be ascertained, by removing plank, or cutting out listings, all fore and aft in either case, that the frame is capable of securely retaining the fastenings.

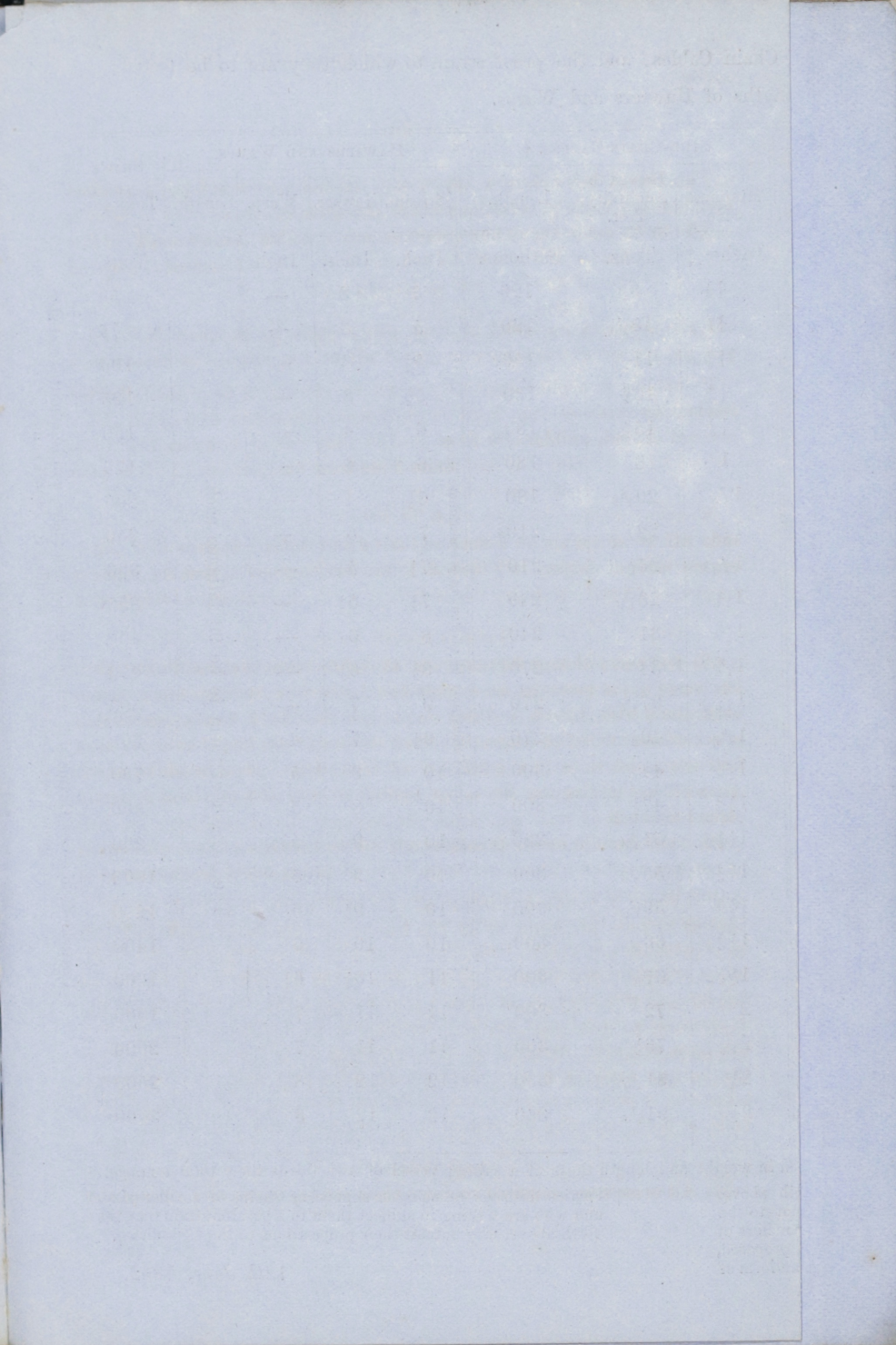
IRON-FASTENED SHIPS.

69. All ships although iron-fastened (except as hereinafter mentioned) shall be classed in the same manner as copper-fastened ships, so long as they remain unsheathed with copper, provided they are, in all other respects, constructed in accordance with the Rules; but when sheathed with copper over the iron fastenings, the words “Coppered over Iron Bolts” shall be added to the character in the Register Book, and continued until the ship be thoroughly copper-fastened.

70. Ships built in India, although fastened with iron, shall be permitted to be copper-sheathed without any mark being placed in the Book, provided the bottom be felted or chunamed and wood-sheathed, and subjected to a careful examination of the iron fastenings on every occasion on which the sheathing is stripped off, for which purpose some of the bolts and nails are to be taken out of the lower part of the bottom, and to be seen by the Surveyor; but no such ship shall be permitted to continue either on the A or on the A in red class for a longer period than one-half the number of years beyond the term originally assigned for her remaining on the A character, unless the bottom shall have been doubled, or the whole of the iron fastenings taken out or properly secured, and the bottom refastened with bolts, or tree-nails, or both, including the middle line, breasthook, and crutch bolts.

EQUIPMENT.

71. All vessels are required to have their masts, spars, and rigging, the



No. 22. Minimum Weights of Anchors; Sizes and Lengths of Chain Cables; and the proof strain to which they are to be tested;
and Sizes and Lengths of Hawasers and Warps.

SHIP'S TONNAGE.	ANCHORS, WITH EITHER IRON OR WOOD STOCKS.							STUD-CHAIN CABLES. †			HAWSERS AND WARPS.				SHIP'S
	Number.			Weight.				Minimum Size	Proved to Admiralty Test.	Length.	Stream.	Hawser.	Warp.	Length.	TONNAGE.
	Bowers.	Stream.	Kedges.	Bowers*	Stream.	Kedge.	2nd Kedge.								
Tons.				Cwts.	Cwts.	Cwts.	Cwts.	Inches. ‡	Tons.	Fathoms.	Inch.	Inch.	Inch.		Tons.
50	2	1	1	3	1	$\frac{1}{2}$	—	$\frac{1}{16}$	$8\frac{1}{2}$	120	5	3	—		50
75	2	1	1	4	$1\frac{1}{2}$	$\frac{3}{4}$	—	$\frac{1}{8}$	$10\frac{1}{10}$	120	5	3	—		75
100	2	1	1	$5\frac{1}{2}$	$1\frac{3}{4}$	1	—	$\frac{1}{8}$	$11\frac{9}{10}$	150	$5\frac{1}{2}$	3	—		100
125	2	1	1	7	2	1	—	$\frac{1}{8}$	$13\frac{3}{4}$	180	$5\frac{1}{2}$	$3\frac{1}{2}$	—		125
150	2	1	1	$8\frac{1}{2}$	$2\frac{1}{2}$	$1\frac{1}{4}$	—	$\frac{1}{8}$	$15\frac{3}{4}$	180	6	4	—		150
175	2	1	1	10	$2\frac{3}{4}$	$1\frac{1}{2}$	—	1	18	180	6	4	—		175
200	3	1	1	11	3	$1\frac{1}{2}$	—	$1\frac{1}{16}$	$20\frac{3}{10}$	180	$6\frac{1}{2}$	4	—		200
250	3	1	2	14	$4\frac{3}{4}$	$2\frac{1}{4}$	1	$1\frac{1}{16}$	$22\frac{3}{4}$	210	7	5	—		250
300	3	1	2	$16\frac{1}{2}$	5	$2\frac{1}{2}$	$1\frac{1}{4}$	$1\frac{3}{16}$	$25\frac{1}{2}$	210	$7\frac{1}{2}$	$5\frac{1}{2}$	—		300
350	3	1	2	19	6	3	$1\frac{1}{2}$	$1\frac{4}{16}$	$28\frac{1}{10}$	240	$7\frac{1}{2}$	$5\frac{1}{2}$	—		350
400	3	1	2	21	$6\frac{1}{2}$	$3\frac{1}{4}$	$1\frac{3}{4}$	$1\frac{5}{16}$	31	240	8	6	—		400
450	3	1	2	23	7	$3\frac{1}{2}$	$1\frac{3}{4}$	$1\frac{6}{16}$	34	270	$8\frac{1}{2}$	$6\frac{1}{2}$	—		450
500	3	1	2	25	8	4	2	$1\frac{7}{16}$	$37\frac{2}{10}$	270	9	7	—		500
600	3	1	2	29	9	$4\frac{1}{2}$	$2\frac{1}{4}$	$1\frac{8}{16}$	$40\frac{1}{2}$	270	$9\frac{1}{4}$	7	4		600
700	3	1	2	32	10	5	$2\frac{1}{2}$	$1\frac{9}{16}$	44	300	10	8	5		700
800	3	1	2	35	$10\frac{1}{2}$	$5\frac{1}{4}$	$2\frac{3}{4}$	$1\frac{10}{16}$	$47\frac{1}{2}$	300	10	8	5		800
900	3	1	2	37	11	$5\frac{1}{2}$	$2\frac{3}{4}$	$1\frac{11}{16}$	$51\frac{2}{10}$	300	10	9	$5\frac{1}{2}$		900
1000	3	1	2	40	12	6	3	$1\frac{12}{16}$	$55\frac{1}{10}$	300	10	9	$5\frac{1}{2}$		1000
1200	3	1	2	43	13	$6\frac{1}{2}$	$3\frac{1}{4}$	$1\frac{13}{16}$	$59\frac{1}{10}$	300	10	$9\frac{1}{2}$	6		1200
1400	3	1	2	45	$13\frac{1}{2}$	$6\frac{3}{4}$	$3\frac{1}{4}$	$1\frac{14}{16}$	$63\frac{1}{4}$	300	10	10	6		1400
1600	3	1	2	47	14	7	$3\frac{1}{2}$	$1\frac{15}{16}$	$67\frac{1}{2}$	300	11	$10\frac{1}{2}$	$6\frac{1}{2}$		1600
1800	3	1	2	49	$14\frac{1}{2}$	$7\frac{1}{4}$	$3\frac{1}{2}$	2	72	300	11	11	7		1800
2000	4	1	2	52	15	$7\frac{1}{2}$	$3\frac{3}{4}$	$2\frac{1}{16}$	$76\frac{1}{2}$	300	11	11	7		2000
2500	4	1	2	57	17	$8\frac{1}{2}$	$4\frac{1}{4}$	$2\frac{1}{8}$	$81\frac{3}{10}$	330	12	12	8		2500
3000	4	1	2	63	19	$9\frac{1}{2}$	$4\frac{3}{4}$	$2\frac{1}{4}$	$91\frac{1}{10}$	360	12	12	8		3000

MEM.—For Steamers the Anchors and Cables will not be required to exceed in weight and length those of a sailing vessel of two-thirds their total tonnage.

* Two of the Bower Anchors must not be less than the weight set forth above, but the third Anchor may be lighter, the reduction in weight however not to be more than 20 per cent. In cases where Ship Owners are desirous of supplying Anchors of lighter weight, application may be made to the Committee, specifying the weight proposed, and setting forth the reasons for such deviation from the rules, whether from the form of the Ship, or from the description of Anchor to be used.

† Parties desirous of using or supplying Chains of smaller size, and who are willing to subject them to a greater strain than set forth above, may submit their propositions to the Committee.

12th June, 1862.

+ Unstudded close-link Chains of 1 inch in diameter and under, will be admitted as Cables, if proved to two-thirds the Test required for Stud-Chains.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

ANCHORS AND CABLES.

NOTICE is hereby given that in pursuance of a Resolution passed this day by the Committee, the Table No. 22, prescribing weights of anchors, &c., and bearing date 27th March last, has been altered, by adding 10 per cent. to the weight therein set forth for weight of anchors.

The following has also been appended as a foot-note to the Table, viz. :—

“Two of the Bower Anchors must not be less than the weight set forth above, but the third Anchor may be lighter, the reduction in weight however not to be more than 20 per cent. In cases where Ship Owners are desirous of supplying anchors of lighter weight, application may be made to the Committee, specifying the weight proposed, and setting forth the reasons for such deviation from the rules, whether from the form of the Ship, or from the description of anchor to be used.”

Copies of the amended Table, No. 22, may be had on application at this office, or from the Surveyors at the several outposts.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

THE HISTORY OF THE UNITED STATES

CHAPTER I

The first chapter of the history of the United States is the discovery of the continent by Christopher Columbus in 1492. This event marked the beginning of European settlement in North America.

The second chapter of the history of the United States is the early years of settlement, from 1492 to 1600. During this period, the first permanent English colony was established in Jamestown, Virginia.

The third chapter of the history of the United States is the period of the American Revolution, from 1763 to 1789. This period was characterized by the struggle for independence from British rule, culminating in the signing of the Declaration of Independence in 1776 and the adoption of the Constitution in 1787.

The fourth chapter of the history of the United States is the period of the Civil War, from 1861 to 1865. This period was marked by the conflict between the Union and the Confederacy over the issue of slavery.

The fifth chapter of the history of the United States is the period of Reconstruction, from 1865 to 1877. This period was characterized by the efforts to rebuild the South and integrate African Americans into society.

The sixth chapter of the history of the United States is the period of the Progressive Era, from 1890 to 1914. This period was marked by the movement for social and political reform.

The seventh chapter of the history of the United States is the period of the Great Depression, from 1929 to 1945. This period was characterized by the economic crisis and the subsequent New Deal policies.

rudder, pumps, windlass or capstan, scuppers, and hawse pipes, in good order, and sails in sufficient number and in good condition.

72. Every ship is to be supplied with a stream-cable or hawser of sufficient size and length, and with at least one good warp; and all vessels are to be provided with anchors and cables of approved quality, properly tested, in number and length, as set forth in the Table, No. 22, annexed.

A Certificate of all new Chains having been tested, and of the strain applied to them, must be produced before a Ship is classed. The amount of strain applied should be marked on each length.

73. The length and condition of the Chain Cables are to be ascertained by removal from the lockers on every Special Survey for Classification.

74. In all cases where hempen cables are used, one-sixth more in length will be required.

BOATS.

75. All vessels under 150 tons to be provided with one good Boat; and every vessel of 150 tons and above to have a suitable number.

76. The efficient state and condition of the whole of the ships' equipment will be designated by the figure 1; and where the same are found insufficient in quantity, or defective in quality, by the figure 2.

SHIPS NAVIGATED BY STEAM.

77. Steam ships are to be subject to the same periodical surveys as sailing vessels, and whenever the boilers are taken out, the vessel is to be submitted to a particular and special survey, in order to ascertain her general condition.

78. That with respect to the Boilers and Machinery, the Owners are required to produce to the Surveyors at the above-directed surveys, a certificate from some competent *Master Engineer*, describing their state and condition at those periods; and to which certificate it is desirable there should be added a description of the particulars of the same, as far as may be practicable, in the manner and form annexed, No. 8; to be appended to the report of survey, and delivered to the Committee, who will thereupon insert in the Register Book the letters "M.C." denoting that the boilers and machinery have been inspected and certified to be in good order and safe working condition; but if no certificate of their condition be furnished by the Owner or Master, then no character can be assigned for the machinery.

79. HULL:—The Surveyors are directed to examine and report the scantling of timbers, plank, and fastenings, and to state where built, and by whom, in the same manner as directed for sailing vessels.

80. The Surveyors are required to report the number, size, length, fasten-

ings, and mode of arrangement of the engine and boiler *sleepers*, and the description of timber of which they are composed, and whether diagonally trussed with wood or iron, and to what extent; the length, size, and fastenings of shelf-pieces and paddle-beams; and whether the vessel be constructed with sponcings, and how they are formed; and to give the length and shifting of the plank outside and inside.

81. **MATERIALS AND EQUIPMENT:**—The Surveyors are to examine and report the number and description of the masts, sails, anchors, cables, hawsers, warps, and boats, as directed to be done for sailing vessels; but the anchors and cables will not be required to exceed in weight and length those of a sailing vessel of two-thirds of the total tonnage of the steam-vessel.

82. The Surveyors are to be particular in examining and reporting the condition of the boats of all vessels employed in carrying passengers.

The Rules, Sections 39, 40, 46, 60, 68, and 72, for Wood Ships, and Sections 10 and 17, for Iron Ships, having been amended or altered since the last edition of the work, attention is particularly directed thereto, also to the amended Table G.

A resolution has been passed, allowing copies to be granted of the original Reports on Ships (if they be already classed, but not otherwise), so far as relates to the Dimensions, Scantlings, Fastenings, and Materials, in cases where the correctness of the Reports in these particulars is certified by the builders. The charge for such copies (which may be obtained on application at this Office) is £1. 1s. each.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, *White Lion Court, Cornhill.*

1st July, 1862.

For Mouldings,
see foot note.

TONNAGE	TONS....	50
*TIMBER AND SPACE	INCHES..	18
Floors, sided and moulded at Keelson, if squared		7
Double Floors, sided and moulded at Keelson, if squared....		6
1st Futtocks, sided and moulded at Floor Heads, if squared..		6
2nd Futtocks, sided, if squared		5½
3rd Futtocks and Long Top Timbers, sided, if squared.....		5½
Top Timbers (Short) sided, if squared .....		..
Top Timbers, moulded at heads, if squared		4
Breasthooks & Wing Transom, sided & moulded in the middle		8
**Keel, Stem, Apron, and Sternpost, sided and moulded		8
Keelson, also the Mainpiece of Rudder from lower part of Counter upwards, sided and moulded		9
†Wales.....		3
‡Bottom Plank, from Keel to Wales.....		2
Sheer Strakes, Topsides, Upper Deck Clamp where there is no Shelf fitted, and Lower Deck Clamp with a Shelf		2¼
Ceiling below Hold Beam Clamp		1½
§Waterway, { Hardwood.....		3½
{ Fir		4
Ceiling betwixt Decks		1½
Bilge Plank, inside, Thick Strakes over long and short Floorheads, and Limber Strake		2½
Lower Deck Clamp where there is no shelf fitted, and Spirketting .....		..
Upper Deck Clamp where a shelf is also fitted.....		2
Planksheer		2
Flat of Upper Deck		2½
Scarphs of Keelson without Rider	ft. in.	4 6
Ditto, where Rider Keelson is added, also Scarphs of Keel..		4 0

of Iron
rkman-
Name.

em, Stern
peller

Frames,
, &c.

M

** The rabbet of the Keel, Stem, and Sternpost

‡ All the

§ This Depth of Waterway for Faying Surface against

MEM.—For relaxations in respect to Poops,

TABLE A.

EXHIBITING THE NUMBER OF YEARS ASSIGNED TO THE DIFFERENT DESCRIPTIONS OF TIMBER, OF GOOD QUALITY, AS USED IN THE TIMBERING AND PLANKING OF SHIPS.

		TIMBERING.									Rudder and Windlass. — Main Pieces.	OUTSIDE PLANK.					INSIDE PLANK.			
		Floors.	First Foothooks.	Second Foothooks.	Third Foothooks and Top Timbers.	Main and Rider Keelsons.	Stem and Stern Post.	Transoms, Knight-heads, Hawse-Timbers, Apron, and Deadwood*	Beams and Hooks.	Knees.		Keel to First Futtock Heads.	First Futtock Heads to Light Mark	Light Mark to Wales.	Wales, Black-Strakes, Topsides, and Sheer-strakes.	Upper deck Waterway, Spirk'ting, and Planksh'rs.	Limber Strakes Bilge Futtrakes, and Ceiling between them	Ceiling in Hold above Bilges and between Decks.	Shelf Pieces Clamps, & Lower Dk Waterway	
1	English, African, and Live Oak, East-India Teak, Morung Saul, Greenheart, Morra, & Iron Bark	12	12	12	12	12	12	12	12	12	12	12	12	12	12	12	12	12	English, African, and Live Oak, East-India Teak, Morung Saul, Greenheart, Morra, & Iron Bark	1
2	Adriatic, Italian, and French Oak, Mahogany of Hard Texture, Cuba Sabicu and Pencil Cedar	10	10	10	10	10	10	10	12	12	10	12	12	10	10	10	12	12	Adriatic, Italian, and French Oak, Mahogany of Hard Texture, Cuba Sabicu and Pencil Cedar	2
3	Spanish Oak	10	10	9	9	10	9	9	10	10	9	12	12	10	9	10	12	12	Spanish Oak	3
4	†Red Cedar, Angelly, and Venatica	9	9	9	9	10	9	9	10	10	—	12	12	12	10	10	12	12	†Red Cedar, Angelly, and Venatica	4
5	Other Continental W. Oak, Span. Ches., Stringy Bark, & Blue Gum	9	9‡	7	7	9	7	7	8	8	7	12	12	9	8	9	10	10	Other Continental W. Oak, Span. Ches., Stringy Bark, & Blue Gum	5
6	North American White Oak and American Sweet Chesnut.....	8	8‡	7	7	8	7	7	7	7	7	12	10	8	7	7	9	9	North American White Oak and American Sweet Chesnut	6
7	Larch, Hackmatack, Tamarac, and Juniper	7	7	7	7	8	7	7	7	7	7||	12	10	8	7	10	7	8	Larch, Hackmatack, Tamarac, and Juniper	7
8	Pitch Pine	7	7	7	7	9	7	7	8	8	7||	12	12	9	9	10	9	9	Pitch Pine	8
9	Second-hand English Oak, African Oak, and East-India Teak	7	7	6	6	6	5	6	6	6	5	—	—	—	—	5	5	5	Second-hand English Oak, African Oak, and East-India Teak	9
10	Cowdie	6 ¶	6	6	7	7	6	6	7	7	—	10	9	8	7	10	7	8	Cowdie	10
11	Baltic and American Red Pine....	5	5	5	7	7	5	5	7	7	5||	9	9	8	7	10	7	8	Baltic and American Red Pine	11
12	English Ash	7	6	5	5	5	4	4	5	5	5||	10	7	4	—	—	5	5	English Ash	12
13	Foreign Ash	5	5	4	4	5	4	4	5	5	—	10	7	4	—	—	5	5	Foreign Ash	13
14	American Rock Elm	6 ¶	6	5	5	6	5	5	5	5	4	12§	8	6	5	5	7	6	American Rock Elm	14
15	European and American Grey Elm	5	5	4	4	4	4	4	5	5	—	12§	8	5	4	4	5	5	European and American Grey Elm	15
16	Black Birch	5 ¶	5**	4	4	4	4	4	4	4	4||	10	7	4	4	4	5	5	Black Birch	16
17	Spruce Fir	5	5**	4	4	4	4	4	4	7	4||	6	6	5	4	4	5	5	Spruce Fir	17
18	English Beech	5 ¶	4	—	—	4	—	—	—	—	4||	12§	8	4	—	—	5	5	English Beech	18
19	Yellow Pine	—	—	—	4	4	4	4	4	4	—	6	5	5	5	5††	5	5	Yellow Pine	19
20	Hemlock	4	4	4	4	—	—	—	4	4	—	4	4	4	4	4	4	4	Hemlock	20

* This Table applies as to the Deadwood so far as regards the Material to be used from the height of two feet above the rabbet of the Keel. † Live Oak and Red Cedar admitted alternately in Timbers of the Frame for 10 A.

‡ If the First Foothooks run up above the Light Watermark, the use of Foreign White Oak is allowed for the 7 years' grade only.

§ The use of Elm and Beech, in Ships above the 8 years' grade, to be restricted to a height from the lower part of the Main Keel, of one-third of the internal depth of the Ship measured, in midships, from the top of the Limber Strake to the top of the Upper Deck Beams.

¶ Black Birch, Beech, American Rock Elm, and Cowdie, allowed for Floors in Midships, to an extent not exceeding one-half the entire length of the Keel, in Ships of the 7 years' grade.

** Black Birch and Spruce allowed for First Futtocks amidships, to the same extent in Ships of the 6 years' grade.

†† Yellow Pine allowed for Waterways of Upper Deck in Ships of the 7 years' grade, if properly fastened, as prescribed in Table B, and provided the Beams are well secured independently of the Waterways.

White Cedar allowed for Third Foothooks and Top timbers in Ships of the 7 years' grade. MEM.—The word "English" includes Timber the growth of the United Kingdom.

|| The Materials marked thus || under the head of "Rudder and Windlass," allowed in Ships of 300 Tons and under only.

TABLE B.

MINIMUM DIMENSIONS OF TIMBERS, KEELSON, KEEL, PLANKING, &c.

For Moulding,
see foot note.

TONNAGE .....TONS....	50	100	150	200	250	300	350	400	450	500	550	600	650	700	750	800	850	900	950	1050	1150	1250	1350	1500	1750	2000
*TIMBER AND SPACE .....INCHES..	18	19	20	21½	23	24¼	25¾	27¼	28½	30	30¾	30½	31	31¼	31½	31¾	32¼	32½	32¾	33¼	33½	33½	33¾	34	34½	35
Floors, sided and moulded at Keelson, if squared	7	7½	8	8¾	9½	10¾	11	11¾	12¼	13	13¾	13¼	13½	13¾	13¾	14	14	14¼	14½	14¾	15	15¼	15¼	15½	15¾	
Double Floors, sided and moulded at Keelson, if squared....	6	6½	7	7¾	8½	9¾	10	10½	11¼	12	12¾	12¼	12½	12½	12¾	12¾	13	13	13¼	13½	13¾	14	14¼	14¼	14½	14¾
1st Futtocks, sided and moulded at Floor Heads, if squared..	6	6½	7	7¾	8¼	8¾	9¼	10	10½	11	11¼	11½	11¾	11¾	12	12¼	12¼	12½	12¾	13¼	13½	13¾	14¼	14¼	14½	14¾
2nd Futtocks, sided, if squared	5½	6	6½	7	7½	8	8½	9	9½	10	10¼	10½	10¾	10¾	11	11¼	11¼	11½	11¾	12¼	12½	12¾	13¼	13¼	13½	13¾
3rd Futtocks and Long Top Timbers, sided, if squared.....	5½	5¾	6	6½	7	7¼	7¾	8¼	8½	9	9¼	9½	9¾	9¾	10	10¼	10¼	10½	10¾	11¼	11½	11¾	12¼	12¼	12½	12¾
Top Timbers (Short) sided, if squared .....	..	..	..	..	..	..	..	..	..	9	9¼	9¼	9¼	9½	9½	9¾	9¾	9¾	10	10	10¼	10½	10¾	10¾	11	11¼
Top Timbers, moulded at heads, if squared	4	4½	4¾	5	5	5¼	5½	5¾	6	6	6¼	6¼	6¼	6½	6¾	6¾	7	7	7¼	7¼	7½	7¾	8¼	8¼	8¾	9
Breasthooks & Wing Transom, sided & moulded in the middle	8	8½	9	9¾	10¼	10¾	11¼	12	12½	13	13¼	13¼	13½	13¾	13¾	14	14	14¼	14½	14¾	15	15¼	15¼	15½	15¾	16
**Keel, Stem, Apron, and Sternpost, sided and moulded	8	9	10	10¾	11¼	11¾	12¼	13	13½	14	14¼	14¼	14½	14½	14¾	14¾	15	15	15¼	15½	15¾	16	16¼	16¼	16½	16¾
Keelson, also the Mainpiece of Rudder from lower part of Counter upwards, sided and moulded	9	10	11	11¾	12¼	12¾	13¼	14	14½	15	15¼	15¼	15½	15½	15¾	15¾	16	16	16¼	16½	16¾	17	17¼	17¼	17½	17¾
†Wales.....	3	3½	4	4¼	4¼	4½	4¾	4¾	5	5	5	5	5¼	5¼	5¼	5½	5½	5½	5¾	6	6	6	6¼	6¼	6½	6¾
‡Bottom Plank, from Keel to Wales.....	2	2¼	2½	2¾	3	3¼	3½	3¾	3¾	4	4	4	4	4	4	4¼	4¼	4¼	4½	4½	4½	4½	4½	4½	4¾	5
Sheer Strakes, Topsides, Upper Deck Clamp where there is no Shelf fitted, and Lower Deck Clamp with a Shelf	2¼	2½	3	3¼	3½	3½	3¾	3¾	4	4	4	4	4	4	4¼	4¼	4¼	4¼	4½	4½	4¾	4¾	5	5¼	5½	5½
Ceiling below Hold Beam Clamp	1½	1¾	2	2¼	2½	2¾	2¾	2¾	3	3	3	3¼	3¼	3¼	3¼	3½	3½	3½	3½	3¾	3¾	4	4	4¼	4½	4½
§Waterway, { Hardwood.....	3½	4	4½	5	5	5½	5½	6	6	6½	6½	6½	7	7	7	7	7½	7½	7½	7½	7½	8	8	8½	8½	9
{ Fir	4	4½	5	5½	6	6½	6½	7	7½	8	8	8	8½	8½	8½	8½	9	9	9	9	9	9½	9½	9½	9½	10
Ceiling betwixt Decks	1½	1¾	2	2	2¼	2¼	2¼	2½	2½	2½	2½	2½	2½	2¾	2¾	2¾	2¾	2¾	2¾	2¾	3	3	3	3¼	3½	3½
Bilge Plank, inside, Thick Strakes over long and short Floorheads, and Limber Strake	2½	2¾	3	3¼	3¼	3½	3¾	3¾	4	4	4¼	4¼	4½	4½	4¾	4¾	5	5	5¼	5½	5¾	6	6¼	6¼	6½	7
Lower Deck Clamp where there is no shelf fitted, and Spirketting .....	..	..	3	3¼	3½	3¾	4	4	4¼	4½	4½	4¾	4¾	4¾	4¾	4¾	5	5	5	5¼	5¼	5½	5½	5½	5¾	6
Upper Deck Clamp where a shelf is also fitted.....	2	2¼	2½	2½	2¾	2¾	2¾	2¾	3	3	3	3¼	3¼	3¼	3¼	3½	3½	3½	3½	3¾	3¾	4	4	4¼	4½	5
Planksheer	2	2¼	2½	2¾	3	3¼	3½	3¾	3¾	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4¼	4½	5
Flat of Upper Deck	2½	2½	2½	2½	2½	3	3	3	3	3½	3½	3½	3½	3½	3½	3½	3½	3½	3½	4	4	4	4	4	4	4
Scarpns of Keelson without Rider	ft. in. 4 6	ft. in. 4 9	ft. in. 5 0	ft. in. 5 3	ft. in. 5 6	ft. in. 5 10	ft. in. 6 2	ft. in. 6 6	ft. in. 6 9	ft. in. 7 0	ft. in. 7 0	ft. in. 7 0	ft. in. 7 0	ft. in. 7 0	ft. in. 7 3	ft. in. 7 3	ft. in. 7 3	ft. in. 7 3	ft. in. 7 6	ft. in. 7 6	ft. in. 7 9	ft. in. 7 9	ft. in. 8 0	ft. in. 8 0	ft. in. 8 0	ft. in. 8 0
Ditto, where Rider Keelson is added, also Scarpns of Keel..	4 0	4 3	4 6	4 9	5 0	5 2	5 4	5 6	5 9	6 0	6 0	6 0	6 0	6 0	6 3	6 3	6 3	6 3	6 6	6 6	6 9	6 9	7 0	7 0	7 0	7 0

Moulding of Futtocks and Top Timbers to diminish gradually from size given at Floor Heads to that at Top Timber Heads. See Rule, sec. 38.

* Should the timber and space be increased, the siding of the timbers to be increased in proportion. See Rules, Sec. 39.

** The rabbet of the Keel, Stem, and Sternpost to be made so as to leave sufficient substance of wood to form a substantial back rabbet.

‡ All the fore and after hoods, both outside and inside, may be reduced one-sixth in thickness. Furrers are not allowed in this or in any other part of a ship.

§ This Depth of Waterway for Faying Surface against Timbers is required, below the underside of the Planksheer, to receive in and out through Bolts at alternate Timbers, with alternate through bolts in Shelf and in Clamp where there is no Shelf.

MEM.—For relaxations in respect to Poops, Top-gallant forecastles, and raised quarter decks, see Rules, sec. 38. For requirements for Vessels the length of which shall exceed five times their breadth, see Rules, Secs. 39 and 62.

TABLE C.

SIDING AND MOULDING OF BEAMS.—See 41.

LENGTH OF BEAM amid-ships.	HOLD BEAMS		DECK BEAMS	
	sided and moulded.	moulded at ends.	sided and moulded.	moulded at ends.
Feet	Inches.	Inches.	Inches.	Inches.
10	—	—	4½	3½
11	—	—	5	4
12	—	—	5¼	4¼
13	—	—	5½	4½
14	—	—	5¾	4¾
15	8	6¾	6¼	5¼
16	8½	7	6½	5½
17	8¾	7½	6¾	5¾
18	9¼	7¾	7	5¾
19	9½	8	7¼	6
20	10	8½	7½	6¼
21	10¼	8¾	7¾	6½
22	10½	9	8	6½
23	11	9¼	8¼	6¾
24	11¼	9½	8½	7
25	11¾	9¾	8¾	7¼
26	12	10	8¾	7¼
27	12½	10¼	9	7½
28	12¾	10½	9	7½
29	12¾	10¾	9¼	7¾
30	13	11	9½	8
31	13¼	11¼	9½	8
32	13½	11½	9¾	8¼
33	13¾	11¾	10	8¼
34	14	11¾	10	8½
35	14¼	12	10¼	8½
36	14½	12¼	10¼	8½
37	14¾	12½	10½	8¾
38	15	12½	10½	8¾
39	15¼	12¾	10½	9
40	15½	13	10¾	9

N.B.—The size of Orlop Beams to be the mean of the sizes here prescribed.

Lloyd's Register of Shipping,
16th May, 1861.

The siding and moulding of all the Beams to be the same as those amidships, except those at the after end of the ship, which may be reduced in proportion to their diminished length. When Spruce or Yellow Pine is used for Beams the dimensions are to be increased.—See Rules, Sec. 40.

TABLE D.

SIZES OF BOLTS, PINTLES OF RUDDER, AND TREENAILS.

Section 46.

TONNAGE .....Not exceeding Tons	50	100	150	200	250	300	350	400	450	500	700	900	1350
Heel-Knee, Stemson, and Deadwood Bolts .....Inches	$\frac{14}{16}$	$\frac{15}{16}$	1	1	$1\frac{1}{16}$	$1\frac{2}{16}$	$1\frac{2}{16}$	$1\frac{3}{16}$	$1\frac{4}{16}$	$1\frac{4}{16}$	$1\frac{5}{16}$	$1\frac{6}{16}$	$1\frac{8}{16}$
Bolts in Sister Keelsons, Scarphs of Keel,* Arms of Breast Hooks, Pointers, Crutches, Riders, Hanging and Lodging Knees to Hold or Lower Deck Beams (except in and out Throat Bolts of Hanging Knees, which must be larger), also in and out Bolts of Shelf, Clamp, and Waterway of Hold or Lower Deck Beams, and the in and out Throat Bolts of Upper Deck Hanging Knees.	$\frac{11}{16}$	$\frac{12}{16}$	$\frac{12}{16}$	$\frac{12}{16}$	$\frac{13}{16}$	$\frac{14}{16}$	$\frac{14}{16}$	$\frac{15}{16}$	$\frac{15}{16}$	1	$1\frac{2}{16}$	$1\frac{3}{16}$	$1\frac{4}{16}$
Keelson Bolts (one through Keel at each Floor), Throats of Transoms, Throats of Breasthooks, and Throats of Hanging Knees to Hold or Lower Deck Beams	$\frac{12}{16}$	$\frac{13}{16}$	$\frac{14}{16}$	$\frac{14}{16}$	$\frac{15}{16}$	1	1	$1\frac{1}{16}$	$1\frac{2}{16}$	$1\frac{2}{16}$	$1\frac{3}{16}$	$1\frac{4}{16}$	$1\frac{6}{16}$
Bilge, Limber Strake, and Through Butt Bolts	$\frac{9}{16}$	$\frac{10}{16}$	$\frac{10}{16}$	$\frac{11}{16}$	$\frac{11}{16}$	$\frac{12}{16}$	$\frac{12}{16}$	$\frac{13}{16}$	$\frac{13}{16}$	$\frac{14}{16}$	$\frac{14}{16}$	$\frac{15}{16}$	1
Other Butt Bolts.....	$\frac{9}{16}$	$\frac{10}{16}$	$\frac{10}{16}$	$\frac{10}{16}$	$\frac{11}{16}$	$\frac{11}{16}$	$\frac{11}{16}$	$\frac{12}{16}$	$\frac{12}{16}$	$\frac{12}{16}$	$\frac{12}{16}$	$\frac{13}{16}$	$\frac{14}{16}$
Bolts through heels of cant timbers at fore and after Deadwood. In and out Bolts of Upper Deck Waterway, Shelf and Clamp, also Arms of Hanging and Lodging Knees, except in and out Throat Bolts of Hanging Knees, which must be larger.....	$\frac{10}{16}$	$\frac{11}{16}$	$\frac{11}{16}$	$\frac{11}{16}$	$\frac{12}{16}$	$\frac{13}{16}$	$\frac{13}{16}$	$\frac{14}{16}$	$\frac{14}{16}$	$\frac{14}{16}$	$\frac{15}{16}$	1	$1\frac{2}{16}$
Pintles of Rudder {The Braces of which must extend so as to receive not less than Two Bolts on the Planking on each side.....}	$1\frac{7}{8}$	2	2	$2\frac{1}{4}$	$2\frac{3}{8}$	$2\frac{1}{2}$	$2\frac{5}{8}$	$2\frac{3}{4}$	3	3	$3\frac{1}{4}$	$3\frac{1}{2}$	$3\frac{1}{2}$
Hardwood Treenails	1	1	1	$1\frac{1}{8}$	$1\frac{1}{8}$	$1\frac{1}{8}$	$1\frac{1}{4}$	$1\frac{1}{4}$	$1\frac{1}{4}$	$1\frac{3}{8}$	$1\frac{3}{8}$	$1\frac{3}{8}$	$1\frac{1}{2}$

* NUMBER OF BOLTS IN SCARPHS OF KEEL:—

In Ships of 150 Tons and under 6 Bolts
 „ above 150 Tons and under 500 Tons 7 do.
 „ 500 Tons and above 8 do.

Lloyd's Register of Shipping,
 16th May, 1861.

N.B.—Bolts to be through and clenched, as prescribed in Section 46.

TABLE E.

NUMBER OF HANGING KNEES
Section 41.

Tons.	To Hold Beams.	To Upper Deck Beams.
	PAIRS.	PAIRS.
150	—	4
200	4	6
250	5	7
300	6	8
350	7	9
400	8	10
450	8	11
500	9	12
550	9	13
600	10	14
650	10	15
700	11	16
750	11	17
800	12	18
900	13	20
1000	14	22
1100	15	24
1350	17	26

TABLE F.

MINIMUM DIMENSIONS OF IRON KNEES AND KNEE RIDERS FOR BRITISH NORTH AMERICAN BUILT SHIPS AND FIRE SHIPS.—Section 62.

TONNAGE .....Tons	150	200	250	300	350	400	450	500	550	600	650	700	750	800	900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000
Number of Hanging Knees to Hold or Lower Deck Beams .....Pairs	3*	4	6	8	9	Upwards, one Knee Rider to every Beam, or Knees and Riders as per Section 62																				
Number of Hanging Knees to Upper and Middle Deck Beams .....Pairs	4	6	7	8	9	10	11	12	13	14	15	16	17	18	Upwards, one to every Beam											
Breadth of Knees and Riders to Hold or Lower Deck Beams .....Inches	3	3	3	3	3	3	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{3}{4}$	3 $\frac{3}{4}$	4	4	4 $\frac{1}{4}$	4 $\frac{1}{4}$	4 $\frac{1}{2}$	4 $\frac{1}{2}$	4 $\frac{3}{4}$	4 $\frac{3}{4}$	5	5	5 $\frac{1}{4}$	5 $\frac{1}{4}$	5 $\frac{1}{2}$	5 $\frac{1}{2}$
Breadth of Upper Deck Knees, where there are two Decks, and of Middle Deck Knees, where there are three Decks .....Inches	3	3	3	3	3	3	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{3}{4}$	3 $\frac{3}{4}$	4	4	4 $\frac{1}{4}$	4 $\frac{1}{4}$	4 $\frac{1}{2}$	4 $\frac{1}{2}$	4 $\frac{1}{2}$	4 $\frac{1}{2}$	4 $\frac{3}{4}$	4 $\frac{3}{4}$	4 $\frac{3}{4}$	4 $\frac{3}{4}$
Thickness of Riders at the joints or butts of the Timbers .....Inches	1 $\frac{1}{4}$	1 $\frac{1}{4}$	1 $\frac{1}{2}$	1 $\frac{1}{2}$	1 $\frac{1}{2}$	1 $\frac{1}{2}$	1 $\frac{3}{4}$	1 $\frac{3}{4}$	2	2	2 $\frac{1}{4}$	2 $\frac{1}{4}$	2 $\frac{1}{2}$	2 $\frac{1}{2}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	3	3	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{3}{4}$	3 $\frac{3}{4}$
Thickness of Knees to Lower Deck or Hold Beams and Knee Riders at the Angle of the Throat .....Inches	2 $\frac{1}{2}$	2 $\frac{1}{2}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	3	3	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{3}{4}$	3 $\frac{3}{4}$	4	4	4 $\frac{1}{4}$	4 $\frac{1}{4}$	4 $\frac{1}{2}$	4 $\frac{1}{2}$	4 $\frac{3}{4}$	4 $\frac{3}{4}$	5	5	5 $\frac{1}{4}$	5 $\frac{1}{4}$	5 $\frac{1}{2}$	5 $\frac{1}{2}$
Thickness of Knees to Lower Deck or Hold Beams and Knee Riders at the Throat Bolts.....Inches	1 $\frac{3}{4}$	1 $\frac{3}{4}$	2	2	2 $\frac{1}{4}$	2 $\frac{1}{4}$	2 $\frac{1}{2}$	2 $\frac{1}{2}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	3	3	3	3	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$	3 $\frac{3}{4}$	3 $\frac{3}{4}$
Thickness of Knees to Upper or Middle Deck at the Throat Bolts †Inches	1 $\frac{1}{2}$	1 $\frac{1}{2}$	1 $\frac{3}{4}$	1 $\frac{3}{4}$	2	2	2 $\frac{1}{4}$	2 $\frac{1}{4}$	2 $\frac{1}{2}$	2 $\frac{1}{2}$	2 $\frac{1}{2}$	2 $\frac{1}{2}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	2 $\frac{3}{4}$	3	3	3	3	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{4}$	3 $\frac{1}{2}$	3 $\frac{1}{2}$
Thickness of Hanging Knees (not Riders) at the ends .....Inches	$\frac{5}{8}$	$\frac{5}{8}$	$\frac{3}{4}$	$\frac{3}{4}$	$\frac{3}{4}$	$\frac{3}{4}$	$\frac{7}{8}$	$\frac{7}{8}$	$\frac{7}{8}$	$\frac{7}{8}$	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
Length of Beam Arms of Knees and Knee Riders for Lower Deck or Hold Beams ‡ ...	ft. 2 in. 6	ft. 2 in. 6	ft. 2 in. 9	ft. 2 in. 9	ft. 3 in. 0	ft. 3 in. 0	ft. 3 in. 3	ft. 3 in. 3	ft. 3 in. 3	ft. 3 in. 6	ft. 3 in. 6	ft. 3 in. 6	ft. 3 in. 9	ft. 3 in. 9	ft. 3 in. 9	ft. 3 in. 9	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0	ft. 4 in. 0

NOTE.—The Bolts in all Iron Riders in Hold, to be not more than twenty-one inches apart on the average.

Standards upon the Beams of such Ships are not admitted as substitutes for Hanging Knees below them.

For sizes of Bolts, see Table D.

* Provided the depth of hold be 13ft. or upwards.

† Breadth and thickness of Knees for Upper Deck, where there are Three Decks, may be one sixth less.

‡ Beam Arms of Upper and Middle Deck Knees, may be three inches shorter than those of the Lower Deck.

Side Arms of Hanging Knees not to be less in length, than one and a half the length of their Beam Arms.

Beam Arms of Knees and Knee Riders, which are 3ft. 6in. in length, to have not less than Four Bolts, and shorter than that length, to have not less than Three Bolts.

Side Arms of all Hanging Knees to have at least One Bolt more than in the Beam Arms.

RULES FOR THE BUILDING OF SEA-GOING IRON SHIPS, OF ALL DESCRIPTIONS, WHETHER SAILING OR NAVIGATED BY STEAM.*

1. The whole of the iron to be of good malleable quality, and all plates and beam and angle iron *to be legibly stamped in two places* with the manufacturer's trade mark, or his name and the place where made, and also to be stated in the report of survey; the workmanship to be well executed, and to be submitted to the closest inspection before coating or painting, and any brittle or inferior article to be rejected. It is not intended to prevent the coating of the plates inside in the way of the frames.

Quality of Iron
and Workman-
ship.
Maker's Name.

2. The keel, stem, stern, and propeller posts are to be scarphed or welded together at discretion, and to be in size according to Table G; if scarphed, the length of scarphs to be regulated in the proportion of eight times the thickness given in the table for keels, and the stern posts, and after end of keel, for screw propelled vessels, to be double the thickness of, or twice the sectional area of, the adjoining length of keel (but the siding in no case to be less than the thickness of the keel), and to be tapered fair into the adjoining length of keel. Where the garboard strakes are thicker than required by the rules, and extend to the bottom of the keel, the thickness of the keel may be proportionably reduced, but such reduction not to exceed one-third of the requisitions of the Rule. Where the keel and keelsons are made of several thicknesses of plates, the plates that form the keel to be in thickness, taken together, the same as is required for a solid keel, as per Table G; and the butts of the several plates of which the keel is formed to be carefully shifted from each other, and from the butts of the garboard strakes, which in all cases must also be shifted, so as not to be opposite each other.

Keel, Stem, Stern
and Propeller
Posts.

3. The spacing and dimensions of the ribs or frames, to be as per Table G, and the ribs or frames in as great lengths as possible, and to be fitted close on to the upper edge of the keel, and in all cases to extend to the gunwale, and wherever butted, to have not less than four feet lengths of corresponding angle iron fitted back to back to cover and support the butts and receive the plating. And if welded together, the welds to be perfect, and the shifts not to be less than four feet.

Ribs of Frames,
Spacing, &c.

* The scantlings given in Table G are intended for ships, the length of which measured from the fore part of the stem to the after part of the stern-post, on the range of the upper deck, does not exceed seven times their extreme breadth, or ten times their depth of hold, taken from the upper part of floors to the top of the upper deck beams.

For ships which are intended to exceed in length the above-named proportions, the builders or owners are to submit for the committee's approval their plans, with full particulars, for giving the vessel sufficient additional strength longitudinally, either by doubling or thickening the sheer-strake and increasing the size of the stringer plates, or otherwise.

Floor Plates.

4. The floor plates to be in thickness as per Table G, and to extend beyond the bilge keelsons, and to be in depth at middle line not less than one inch for every foot of the vessel's depth, measured from the top of upper or spar deck beams to top of floor plate, and not to be less in depth at the bilge keelsons than the moulding of the frame. A floor plate to be fitted and rivetted to every frame, and at the ends of the vessel the floor plates to be worked across the middle line so as to support and unite the sides of the vessel efficiently to each other. Watercourses are to be formed through all the floor plates at the limbers on each side of middle line, so as to allow water to reach the pumps freely.

Reversed Angle Iron.

5. Reversed angle iron on frames to be in size as per Table G. All vessels, of whatever size, to have reversed angle iron rivetted to every frame and floor plate across the middle line to the height of upper part of bilges, and to have double reversed angle iron in way of all keelsons; and in addition all vessels of 300 tons and upwards to have reversed angle iron extended from bilges to the upper deck beam stringer on alternate frames, and vessels of 800 tons and upwards to have reversed angle iron extended on every frame from bilges to lower deck or hold beam stringer, if the vessel has two decks or tiers of beams, and to the height of middle deck beam stringer if the vessel has three decks or tiers of beams; the rivets for securing the reversed angle iron to the frames and floor plates to be in diameter equal to those specified in the table for the outside plating, and not to exceed eight times their own diameter apart.

Middle Line Keelson.

6. The middle line keelsons,* if of single plate, to be of the same thickness as the garboard strakes, and if standing above the floor plates to be well fitted and rivetted to the same, and a reversed angle iron to be fitted on each side, both on the top and the bottom, extending all fore and aft, the lower angle irons to be secured to the double reversed angle irons on the top of floors. If box keelsons be adopted, the plating to be of the thickness as per Table G, and in either case to be two-thirds of the depth of floor plates.

If intercostal middle line keelsons be adopted, they are to be of the same thickness as the floor plates, and rivetted to vertical angle irons on all floor plates at each end, the plates to extend from upper edge of keel to above the upper edge of floor plates, sufficiently high to be rivetted between the double angle irons extending all fore and aft, of the dimensions given in the Table G, and the said double angle irons of keelson are to be rivetted to double angle irons on top of all floor plates.

* In all cases the Keelsons, and where practicable, the Shelfpieces and Stringers, are to be carried fore and aft, without being cut off at the Bulkheads, the latter being made watertight around them; and where such parts of the ship are necessarily separated, they are to be efficiently connected, to the satisfaction of the Surveyor.

7. The bilge keelsons to be fitted and secured in an efficient manner, Bilge Keelsons. extending all fore and aft, and placed at lower turn of bilges, according to the form of the bottom. In ships of 1000 tons and upwards, an intercostal keelson to be fitted on each side, fore and aft as far as practicable, about midway between the middle line keelson and the bilge keelson, with double angle iron rivetted on the top of floor plates. All vessels of 500 tons and upwards to have fitted between the bilge keelsons and the hold beams, at the upper part of the turn of bilge, strong angle irons, as stringers, extending all fore and aft, rivetted back to back and to the reversed irons on the frames, the size of them not to be less than those used for the middle line keelson.

8. No plates to be less than nine feet in length, except the fore and after Plating. hoods, and all plates to be well fitted and secured to the ribs and each other, the butts to be closely fitted, and to be united by lining pieces or strips of not less than the same thickness as the plates, and of sufficient breadth for rivetting, as described hereafter. No butts of outside plating to be nearer each other than one space of frames, nor to be nearer to a scarp of keel than that distance.

The space between the outside plating and the frames, to have solid filling pieces closely fitted in one length, of the same breadth as the frames.*

9. In the outside plating, stringer plates upon beams, angle iron on stringer plates, and flat of deck of raised quarter-decks, a reduction of one-fifth from the thickness required by the Table G for such parts in the range of the upper deck in ships with two decks will be allowed. Reductions allowed in raised Quarter Decks, Poops, Forecastles, &c.

In the outside plating, stringer plates upon beams, angle iron on stringer plates, and flat of deck of full poops and top gallant forecastles, a reduction of one-fourth from the thickness required by the Table G for such parts in the range of the upper deck in ships with two decks will be allowed, and for the beams of full poops and top gallant forecastles a reduction of one-fifth will be allowed. The united lengths of poop and forecastle not to exceed three-fifths of the entire length of the upper deck.

In the scantlings of beams, plating, flat of deck, stringer plates, and angle iron on stringers to upper (or spar deck) in vessels with three decks, viz., upper, middle, and lower deck, a reduction of one-sixth from the dimensions given for such parts in the range of upper deck in ships with two decks will be allowed.

10. For the spacing of beams the depth of hold is measured amidship from Depth of Hold, for Space of Beams. the top of the floor plates to the top of the upper deck beams in vessels with

* It is recommended that in all cases the sheerstrake be an outside strake, so as to admit of the butt-straps, or lining-pieces, being extended in one piece, from the foreside of the frame next afore the butts to the aftside of the frame next abaft the butts, or to admit of doubling the sheerstrake where it may be required; also that all butt-straps, or lining-pieces to butts, be fitted with the fibre of the iron in the same direction as the fibre of the plating to which they are rivetted.

two decks, and to the top of the middle deck beams in vessels with three decks.

Beams.

The beams to be of the dimensions as per Table G, and to be made of "bulb," or any other approved iron plates, with reversed angle iron rivetted to the plates, the beams to be well and efficiently connected or rivetted to the corresponding frames at the sides of the vessel, with bracket ends or knee plates of thickness equal to the beams, and in length, as per Table G, also to the stringer plates, the beams of each deck to be over each other, and pillared where practicable.

Upper deck beams in vessels with one or two tiers of beams, and the upper (or spar deck) and middle deck beams in vessels with three tiers of beams, to be fastened to alternate frames.

Vessels of 12 feet and under 13 feet depth of hold, or where the gross register tonnage exceeds 200 tons, shall be required to have as many hold beams as may be practicable or convenient, fastened to, at least, every eighth frame.

Vessels of 13 feet depth and under 15 feet, to have hold beams fastened to every fourth frame.

Vessels of 15 feet depth and under 18 feet, to have hold or lower deck beams fastened to every second and fourth frame, alternately.

Vessels of 18 feet depth and above, to have hold or lower deck beams fastened to every alternate frame, and the same number of middle deck beams, where such are required.

All vessels having two decks (viz., upper and lower deck), and exceeding 24 feet in depth from the top of floor plates to the upper side of upper deck beams, and vessels with three decks, viz., upper (or spar), middle, and lower deck, and exceeding 24 feet in depth to the upper side of middle deck beams, and where the depth from under side of lower deck beams exceeds 15 feet, such ships to have orlop beams fastened to every sixth frame; also to have stringer plates and angle iron on their ends, all fore and aft, *equal in strength* to the requirement at Section No. 15; but, in the case of flush deck ships, a depth of 25 feet will be allowed, provided the lower hold does not exceed 15 feet in depth from the under side of lower deck beams.

Should a house be constructed on a flush deck ship for lodging crew or for store-room, the same not to extend aft beyond 10 feet from the mizen mast.

Where a deviation from the foregoing Rules as applying to beams takes place in way of engine-rooms or hatchways, or where no deck is intended to be laid, and the above named spaces would materially interfere with the stowage of cargo, and where partial or entire bulkheads with horizontal

shelves and stringers between them, or larger beams are substituted for ordinary beams in wider spaces, a sketch with all particulars must be submitted, through the resident surveyor, for the Committee's consideration. The middle deck to be a perfect deck laid and caulked.

11. The rivets to be of the best quality, and to be in diameter as per Table G; the rivet holes to be regularly and equally spaced and carefully punched opposite each other in the laps and lining pieces or strips, to be countersunk all through the outer plating; the rivets not to be nearer to the butts or edges of the plating, lining pieces to butts, or of any angle iron, than a space not less than their own diameter, and not to be further apart from centre to centre than four times their diameter, or nearer than three times their diameter, and to be spaced through the frames and outside plating, and in reversed angle iron, a distance equal to eight times their diameter apart. When rivetted up they are completely to fill the holes, and their points or outer ends are to be round or convex, and not to be below the surface of the plating through which they are rivetted. In vessels intended for the 12 years' grade, of *700 tons and upwards*, all edges or horizontal joints of outside plating to be double rivetted: and in vessels of the 12 years' grade *under 700 tons*, and in *all vessels* of the 9 years' grade, all edges or horizontal joints of outside plating to be double rivetted from the keel to the height of upper part of bilges, all fore and aft. The stem, stern post, keel, edges of garboard strakes and sheer strakes, and butts of outside plating, and butts of floor plates, breasthooks, transoms, and plates of beams, also butts of keelsons, stringers, shelf-plates, and all other longitudinal ties, to be double rivetted in all vessels. The overlaps of plating, where double rivetting is required, not to be less in breadth than five times the diameter of the rivets; and where single rivetting is admitted, the overlaps to be not less in breadth than three times the diameter of the rivets. If double rivetting be adopted where single rivetting is allowed by the Rules, the diameter of the rivets may be reduced one-sixteenth of an inch below that prescribed by the Rules, provided that in no case the diameter be reduced below five-eighths of an inch. The butts and edges of outside plating to be truly fitted, carefully caulked, and made water-tight.

12. In addition to the engine room bulkheads of steamers, all vessels to have two water-tight bulkheads, built at a reasonable distance from the ends, to extend from the keel, and outside plating to the upper deck in vessels with two decks, and to the middle deck in vessels with three decks (otherwise called "tonnage deck"); but it shall not be required to extend the aftermost bulkhead to this height if it be continued above the load water line, and be connected to a water-tight platform or deck of iron extending

from its upper part entirely round the after part of the vessel, thus enclosing the lower after body in a water-tight tank. If a screw shaft passes through a bulkhead, it is to be made water-tight at the bulkhead. All plating of bulkheads to be of the thickness prescribed in Table G, and to be closely fitted between two frames or ribs at each side of the vessel, and strongly rivetted through them, or if attached only to one frame, then to have brackets or knee plates rivetted horizontally against the side plating of the vessel and to the bulkheads, foreside and aftside alternately, near the middle of the outside plates, to be strongly rivetted thereto. Lining pieces between these frames and outside plating in way of bulkheads are to be plates extending in one piece from the foreside of the frame afore the bulkhead frames to the aftside of the frame abaft the bulkhead frames; also the bulkheads to be supported vertically by angle irons of the dimensions given in the Table G, which are not to exceed two feet six inches apart, the whole to be efficiently connected and rivetted together and to the corresponding floors, beams of the several decks, and the frames or ribs. The whole of the bulkheads to be caulked and made thoroughly water-tight. A sluice, cock, or valve, to be fitted at the limbers on each side of middle line, at each water-tight bulkhead, the same to be worked from the deck above so as to allow water to be shut off, or to reach the pumps when required.

Ceiling.

13. The wood ceiling or lining of all vessels from 100 to 3000 tons to be from $1\frac{1}{2}$ inches to 3 inches in thickness, in proportion to the tonnage, and to be so fastened to the reversed angle irons or frames that it may be easily removed for survey and painting.

Decks, Waterways, and Plank-sheers.

14. The waterways and plank-sheers, if of wood, not to be inferior in quality of material to that which is prescribed in Table A for vessels built of wood of the same grade. The flat of upper deck to be fastened by screw bolts put through from the upper side, and to have nuts at the under side of the angle iron of the beams; where the planks exceed six inches in width, two bolts in each plank in every beam, one of which may be a short screw bolt. The waterways to be fastened with screw bolts with nuts at under side of stringer plates.

Stringer plates and tie plates.

15. All vessels to have stringer plates upon the ends of each tier of beams, to be not less in breadth and thickness than the dimensions given in Table G, the said stringer plates to be fitted home and rivetted to the outside plating at all upper decks, and at the middle deck in vessels having three decks, with angle iron of the dimensions given in the Table; and the stringer plates of the middle deck of ships with three decks to have an additional angle iron extending all fore and aft inside of the frames, and rivetted to the reverse angle iron on the frames. All vessels to have upon each tier of beams a

tie-plate* each side the hatchways of the dimensions given in Table G, extending all fore and aft throughout, and well rivetted to the upper sides of all the beams, deck hooks, and transoms. Also to have plates, where practicable, of the same dimensions, extending diagonally from side to side, rivetted to the upper side of beams and stringer plates.

16. The main piece of rudder to be made of the best hammered iron, and so arranged as to ship and unship, where practicable, without docking, and the main piece to be in size, according to the Table G. Rudder.

17. Vessels intended for either the 12, 9, or 6 years' grade to be surveyed at least five times, in the following order, viz :— Surveys.

On the several parts of the frame, when in place, and before the plating is wrought.

On the plating during the progress of rivetting.

When the beams are in and fastened, and before the decks are laid.

Again when the ship is complete, and before the plating is finally coated.

And lastly, after the ship is launched.

All vessels to be subject to occasional or annual survey when practicable, and every third year to be specially surveyed in dry dock or laid on blocks, with both surfaces of outside plating exposed;† and whenever the engines or the boilers of iron steam ships are taken out, the vessel shall be submitted to a particular and special survey.

CONTINUATION OF IRON SHIPS TO THE CHARACTER A.

18. If, on the termination of the period of original designation, or if at any subsequent period, not exceeding one-half the number of years assigned originally, or on restoration, an owner shall wish to have his ship remain or be replaced on the letter A, he is to send a written notice thereof to the Secretary, and the Committee shall then direct a special survey, as follows, to be held by not less than two competent persons, to be appointed by the Committee, one of them to be a surveyor the exclusive servant of the Society :

SURVEY.

The vessel to be placed on high blocks, in a dry dock, or upon ways, and proper stages to be made, so that the rivets and plates of keel, and flat of bottom, may be thoroughly examined; the whole of the ceiling or lining

* Upon hold beams where no deck is intended to be laid, or where such tie-plates would materially interrupt stowage of cargo, an angle iron, of the dimensions given in the Table G for "Angle Iron on Beam Stringers," will be admitted in lieu thereof, placed at the middle line, extending all fore and aft throughout where practicable, and well rivetted to all beams, deck hooks, and transoms.

† As regards ships where the inner surface of the bottom plating is coated with cement or asphalte, instructions have been given to the Society's Surveyors in relaxation of this requirement. Ships which have undergone the above examination will be noted in the Register Book thus (t. s.); and if not submitted to such triennial Survey, will be liable to have their character suspended.

inside to be entirely removed; coal bunkers of steam vessels to be cleared, so as to expose the whole of the frames, stringers, hooks, floor plates, keelsons, engine and boiler bearers, ends of beams, water-tight bulkheads, rivets, and inner surface of the plating, to view; the hold to be cleared; all oxidation to be removed by being cut or beaten off the several parts above-named, also from the outside plating, rivets, keel, stem, sternpost, and rudder, so as to completely lay bare all the surfaces of iron; the plank-sheers and waterways, if of wood, to be scraped bright: and when the vessel is so prepared, the Surveyors are to examine and report the condition and thickness of all the parts of iron above-named, also the condition of the plank-sheers, waterways, flat of decks and their fastenings; and upon the owner consenting to remove and replace with proper materials, equal in substance and quality to the original construction, such parts as may be found defective, or less than three-fourths of the required substance by Rule, such vessel, upon the repairs and efficiency being reported to the Committee, may be continued on the letter A for a term of years not exceeding one-half the number of years assigned originally, or on restoration, subject to occasional or annual survey when practicable. The period of continuation will, upon all occasions, commence from the time the ship may have gone off the letter A, without regard to the date when the survey for this purpose may be held.

RESTORATION OF IRON SHIPS TO THE CHARACTER A.

19. If, *at any age of a vessel*, an owner be desirous to have his ship restored, such restoration, on his application to the Committee, and consenting to the special survey hereinafter described, to be held by two Surveyors, one of whom shall be an exclusive servant of the Society, and performing the repairs thereby found requisite, will be granted for a period not exceeding two-thirds of the time originally assigned, the same to be calculated from the date of such repairs.

Survey and Requisites for Restoration.

The vessel to be placed on high blocks, in a dry dock, or upon ways, and proper stages to be made, so that the rivets and plates of keel, and flat of bottom, may be thoroughly examined; the whole of the ceiling or lining inside to be entirely removed; coal bunkers of steam vessels to be cleared the boilers to be taken out and also the engines (unless it shall be shown by previous survey that the removal is unnecessary), so as to expose the whole of the frames, stringers, hooks, floor plates, keelsons, engine and boiler bearers, ends of beams, water-tight bulkheads, rivets, and inner surface of the plating, to view; the hold to be cleared; all oxidation to be removed by being cut or beaten off the several parts above-named, also from the outside

plating, rivets, keel, stem, sternpost, and rudder, so as to completely lay bare all the surfaces of iron; the planksheers and waterways, if of wood, to be entirely removed, and also the flat of upper deck, except under special circumstances, to be sanctioned by the Committee in each case: and when the vessel is so prepared, the Surveyors are to examine and report the condition and thickness of all the parts of iron above-named, also the condition of the beams and their fastenings; and upon the owner consenting to remove such parts as may be found defective, or objected to, or less in thickness than hereinafter admitted for repairing such vessel, and replace them with proper materials equal in quality and substance to that required in the Table G for the nine years' grade in those originally classed 12 A, and equal in quality and substance to that required in the Table G for the six years' grade in vessels originally classed 9 A or 6 A, such vessel, upon the repairs and efficiency being reported to the Committee, may be restored to the letter A, for a term of years not exceeding two-thirds the number of years assigned originally, subject to occasional survey.

Iron ships, which have been restored under the foregoing rule, shall be entitled to continuation thereon, subject to the same conditions of survey and examination as are prescribed for ships proposed to be continued at the expiration of the period first assigned to them; but in like manner, the term of such extended continuance to be limited to a period not exceeding one-half the number of years for which the ships may respectively have been restored, without reference to the period originally assigned to them.

20. On the expiration of the terms assigned to ships classed A, they will be liable to lapse (like ships built of wood).

21. One year will be added to the character of all ships of the A class built under a roof which shall project at each end beyond the length, and on each side beyond the breadth, a quantity equal to one-half the breadth of the vessel.

22. Vessels not surveyed while building, will be classed A from year to year only, but for a period not exceeding Six Years.

IRON SHIPS ALREADY CLASSED A 1.

Iron ships built prior to the promulgation of the Rules will be allowed to remain in the Register book classed A 1 from year to year, *subject to annual survey*, until the expiration of Six Years from their date of build, and then be examined to determine the period to which they may be entitled under the rules; and if, on such examination, it shall be found the ships are entitled to the 9 or 12 years' grade, it will be in the option of the owners either to adopt such period respectively, or continue the vessel A 1 from year to year,

as above, until the expiration of the extended period; but if it shall be found that the term of years for which a vessel would have been entitled to remain on the A character has expired, she will be classed *Æ*, if entitled thereto, unless specially surveyed for continuation or for restoration.

For Equipment, see Sections 71, 72, 73, 74, 75, and 76, of Wood Ships.

The Rules for the building of Iron Ships having been now for some time before the public, and the principles upon which they are framed having been found generally to work satisfactorily, and the Committee having very carefully revised the several regulations, especially in respect to the thickness of the plating and the extension of double rivetting, beg respectfully to urge upon the builders of iron ships the necessity of a close approximation to the Rules, and a conformity to the Table of Dimensions, so as to justify the Committee in granting the character A for the respective periods for which the ships are built.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

*No. 2, White Lion Court, Cornhill.
London, 22nd May, 1862.*

IRON SHIPS.—TABLE

All

Gross Tonnage.	Keel, Stem, and Stern Post for all Grades.*	Distance of Frames or Ribs from Moulding edge to Moulding edge all fore	FRA
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TABLE G.
IRON SHIPS.—TABLE OF MINIMUM DIMENSIONS OF FRAMES, PLATING, RIVETS, KEELS, KEELSONS, STEMS, STERN POSTS, FLOOR PLATES, BEAMS, BULKHEADS, STRINGERS, &c.
 All plates, and all beam and angle iron, used in ships intended for classification, are to be stamped legibly in two places with the manufacturer's trade mark, or his name and the place where made.

Gross Tonnage.	Keel, Stem, and Stern Post for all Grades.*	Distance of Frames or Ribs from Moulding edge to Moulding edge all fore and aft for all Grades.	FRAMES OR RIBS. Dimensions of Angle Iron for all Grades.	Dimensions of Reversed Angle Iron on Frames and Bulkheads for all Grades.	THICKNESS OF PLATES.†									Thickness of Plates for Beams‡ and Bulkheads for all Grades.		Dimensions of Angle Iron on Beam Stringers or Keelsons for all Grades.	RUDDER for all Grades.		Thickness of Wood Flat of Upper Deck.	Gross Tonnage.
					Garboard Strakes* and Single Plate Middle Line Keelsons standing upon floors.			From the Garboard to the upper part of Bilge, and the Sheerstrakes.*			From Bilge to Sheerstrakes, thickness of † Stringer Plates upon Beam Ends, Hooks, Crutches, †† Floor Plates, and Box or intercostal Keelsons.						Diameter at the Head.	Diameter at the Heel.		
					Years.			Years.			Years.									
	Inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.	inches.		
100	5½×1½	18	6/16×2½×2½	5/16×2¼×2¼	8/16	7/16	6/16	7/16	6/16	5/16	6/16	5/16	5/16	5/16	4/16	6/16×2½×2½	2½	2	2½	100
200	6×2	18	6/16×3×2½	5/16×2¼×2¼	9/16	8/16	7/16	8/16	7/16	6/16	7/16	6/16	5/16	5/16	4/16	6/16×3×2½	3	2	2½	200
300	6¼×2¼	18	6/16×3¼×2¾	5/16×2½×2½	10/16	9/16	8/16	9/16	8/16	7/16	8/16	7/16	6/16	6/16	5/16	6/16×3½×2¾	3¼	2¼	3	300
400	6½×2½	18	7/16×3½×2¾	6/16×2¾×2½	10/16	9/16	8/16	9/16	8/16	7/16	8/16	7/16	6/16	6/16	5/16	6/16×4×3	3½	2½	3	400
500	6¾×2½	18	7/16×3¾×2¾	6/16×3×2½	11/16	10/16	9/16	10/16	9/16	8/16	9/16	8/16	7/16	7/16	6/16	7/16×4¼×3¼	3¾	2¾	3½	500
600	7×2¾	18	7/16×4×3	6/16×3×2¾	11/16	10/16	9/16	10/16	9/16	8/16	9/16	8/16	7/16	7/16	6/16	7/16×4½×3½	4¼	2¾	3½	600
700	7¼×2¾	18	8/16×4¼×3	7/16×3×2¾	12/16	11/16	10/16	11/16	10/16	9/16	10/16	9/16	8/16	8/16	6/16	8/16×4¾×3¾	4½	3	3½	700
800	7½×3	18	8/16×4½×3	7/16×3×3	12/16	11/16	10/16	11/16	10/16	9/16	10/16	9/16	8/16	8/16	6/16	8/16×5×4	4½	3	3½	800
900	8×3	18	8/16×4¾×3	7/16×3¼×3	13/16	12/16	11/16	12/16	11/16	10/16	11/16	10/16	9/16	9/16	7/16	9/16×5×4¼	4¾	3	3½	900
1000	8½×3	18	9/16×5×3	8/16×3½×3	14/16	12/16	11/16	12/16	11/16	10/16	11/16	10/16	9/16	9/16	7/16	9/16×5×4½	5	3	4	1000
1200	9×3	18	9/16×5×3½	8/16×3½×3	15/16	13/16	12/16	13/16	12/16	11/16	11/16	10/16	9/16	9/16	7/16	9/16×5½×4½	5	3¼	4	1200
1500	10×3	18	10/16×5½×3½	9/16×4×3½	16/16	14/16	13/16	14/16	13/16	12/16	12/16	11/16	10/16	10/16	8/16	9/16×6×5	5½	3½	4	1500
2000	12×3	18	10/16×6×4	9/16×4½×3½	17/16	15/16	14/16	15/16	14/16	13/16	13/16	12/16	11/16	11/16	8/16	10/16×6½×5½	6	3¾	4	2000
2500	12×3¼	18	11/16×6¼×4	10/16×4½×3½	17/16	16/16	15/16	16/16	15/16	14/16	14/16	13/16	12/16	12/16	8/16	10/16×6½×5½	6½	4	4	2500
3000	12×3½	18	11/16×6½×4	10/16×4½×3½	17/16	16/16	15/16	16/16	15/16	14/16	14/16	13/16	12/16	12/16	9/16	10/16×6½×5½	6¾	4½	4	3000

MEM.—The Scantlings given in the above Table are intended for Ships the length of which, measured from the fore part of the Stem to the after part of the Stern-post on the range of the Upper Deck, does not exceed seven times their extreme breadth, or ten times their depth of Hold, taken from the upper part of Floors to the top of the Upper Deck Beams. For Ships which are intended to exceed in length the above-named proportions, the Builders or Owners are to submit for the Committee's approval their plans, with full particulars for giving the Vessel sufficient additional strength longitudinally, either by doubling or thickening the Sheerstrake and increasing the size of Stringer Plates, or otherwise.

RIVETS. Diameter of Rivets required for Thickness of Plates - - -	5/8 of an Inch.			¾ of an Inch.			7/8 of an Inch.			1 Inch.			Rivets to be ¼ of an inch larger in diameter in the stem, stern-post, and keel.
	5/16	6/16	7/16	8/16	9/16	10/16	11/16	12/16	13/16	14/16	15/16	16/16	

* Hollow or flat keel plates and garboard strakes, and main sheerstrakes, are not to be less in breadth than as follows, viz.:—In ships under 500 tons, 2ft.; in ships 500 and under 1000 tons, 2ft. 6in.; in ships 1000 tons and upwards, 3ft. When Hollow Plate Keels are adopted, their thickness should not be less than one and a half that of the Garboard Strake. For Keels of other Forms, see Sec. 2.

† No plates to be less than nine feet in length, except the fore and after hoods; and the Plating not to be reduced in thickness forward or aft, except in the sheerstrake and strake next below it, which may be reduced 1/16 of an inch in Vessels of 1000 Tons and under, and 1/8 of an inch in Vessels above 1000 Tons, for a distance not exceeding one quarter of the length of the Vessel from each end.

‡ All Beam Plates to be in depth one quarter of an inch for every foot in length of the Midship Beam; to have double Angle Iron upon upper edge, Siding & Moulding together of each to be not less than three-fourths the depth of Beam Plate, and to be in thickness 1/16 of an inch for every inch of the two sides of the Angle Iron.

§ Stringer Plates upon ends of Beams not to be less in breadth than three times the depth of Beams, and to be of the thickness given in the Table, the said Stringer Plates to be fitted home and rivetted to the outside plating at all Upper Decks, and at the Middle Deck in Vessels having three decks, with Angle Iron of the dimensions given in the Table above. The Plates ranging all fore and aft upon Beams on each side of Hatchways, or from side to side diagonally, to be half the width, and of the same thickness as the Stringer Plates upon ends of Beams. Each arm of Knee Plates not to be less in length than twice and half the depth of the Beams.

|| Depth of Floor Plates at the middle line not to be less than one inch for every foot of the Vessel's depth, measured amidship from the top of the Floor Plates to the top of the Upper or Spar Deck Beams, to extend beyond the Bilge Keelsons, and not to be less in depth at the Bilge Keelsons than the Moulding of the Frames.

The Rivets to be of the best quality, and to be in diameter as per Table; the rivet holes to be regularly and equally spaced and carefully punched opposite each other in the laps and lining pieces, or strips; to be countersunk all through the outer Plating, the rivets not to be nearer to the Butts or edges of the Plating, Lining Pieces to Butts, or any Angle Iron, than a space not less than their own diameter, and not to be further apart from centre to centre than four times their diameter, or nearer than three times their diameter, and to be spaced through the Frames and outside Plating, and in reversed Angle Iron, a distance equal to eight times their diameter apart. When rivetted up, they are completely to fill the holes, and their points, or outer ends, are to be round or convex, and not to be below the surface of the Plating through which they are rivetted. In vessels intended for the 12 years' grade, of 700 tons and upwards, all edges or horizontal joints of outside plating to be double rivetted; and in vessels of the 12 years' grade under 700 tons and in all vessels of the 9 years' grade, all edges or horizontal joints of outside plating to be double rivetted from the keel to the height of upper part of bilges, all fore and aft. The Stem, Stern Post, Keel, Edges of Garboard Strakes and Sheerstrakes, and Butts of Floor Plates, Breasthooks, Transoms, and Plates of Beams; also butts of Keelsons, Stringers, Shelf Plates, and all other longitudinal ties, to be double rivetted in all Vessels. The overlaps of Plating where double rivetting is required, to be not less in breadth than five times the diameter of the rivets, and where single rivetting is admitted, the overlaps to be not less in breadth than three times the diameter of the rivets. If double rivetting be adopted where single rivetting is allowed by the Rules, the diameter of the rivets may be reduced 1/16 of an inch below that prescribed by the Rules, provided that in no case the diameter be reduced below 5/8 of an inch.

MEM.—Ships built under survey, in which the thickness of the plating is equal to the requirements of the Rules, but which, from partial deficiencies, may not appear to be in all respects entitled to the full terms of years contemplated by the above Table, will have one year, at the discretion of the Committee, abated from the period to which they would otherwise be entitled.

Lloyd's Register of Shipping, London, 17th April, 1862.

No. 1.—A TABLE exhibiting the different Descriptions of TIMBER, of good to the several Terms of Years appointed

PARTS OF THE FRAME OF A VESSEL.	TWELVE YEARS.	TEN YEARS.	NINE YEARS.	EIGHT YEARS.
FLOORS	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Adriatic, Italian, Spanish, and French Oak.	The same as in the preceding Class, and admit †Red Cedar, Angelly and Venatica Other Continental White Oak Spanish Chesnut, Stringy Bark and Blue Gum.	The same as in the preceding Class, and admit North American White Oak American Sweet Chesnut.
1st FUTTOCKS	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Adriatic, Italian, Spanish, and French Oak.	The same as in the preceding Class, and admit †Red Cedar, Angelly and Venatica Other Continental White Oak Spanish Chesnut, Stringy Bark and Blue Gum.	The same as in the preceding Class, and admit †North American White Oak American Sweet Chesnut.
2d FUTTOCKS..	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture Adriatic, Italian, and French Oak Cuba Sabicu Pencil Cedar.	The same as in the preceding Class, and admit Spanish Oak †Red Cedar, Angelly and Venatica.	The same as in the preceding Class.
3d FUTTOCKS.. and TOP TIMBERS	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture Adriatic, Italian, and French Oak Cuba Sabicu Pencil Cedar.	The same as in the preceding Class, and admit Spanish Oak †Red Cedar, Angelly and Venatica.	The same as in the preceding Class.
MAIN and RIDER KEELSONS....	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture—Cuba Sabicu Pencil & Red Cedar Angelly & Venatica Adriatic, Italian, Spanish & French Oak.	The same as in the preceding Class, and admit Other Continental White Oak Spanish Chesnut, Stringy Bark, Blue Gum and Pitch Pine.	The same as in the preceding Class, and admit North American White Oak American Sweet Chesnut Larch Hackmatack Tamarac—Juniper.
STEM	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture Adriatic, Italian, and French Oak Cuba Sabicu Pencil Cedar.	The same as in the preceding Class, and admit Spanish Oak †Red Cedar, Angelly and Venatica.	The same as in the preceding Class.
and STERN POSTS	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture Adriatic, Italian, and French Oak Cuba Sabicu Pencil Cedar.	The same as in the preceding class, and admit Spanish Oak †Red Cedar, Angelly and Venatica.	The same as in the preceding Class.
TRANSOMS.... KNIGHTHEADS HAWSE TIMBERS..... APRON and *DEADWOOD ..	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture Adriatic, Italian, and French Oak Cuba Sabicu Pencil Cedar.	The same as in the preceding class, and admit Spanish Oak †Red Cedar, Angelly and Venatica.	The same as in the preceding Class.
BEAMS	English, African, Adriatic, Italian, French, and Live Oak—East-India Teak—Morung Saul—Greenheart—Morra—Iron Bark—Mahogany of hard texture Cuba Sabicu Pencil Cedar.	The same as in the preceding Class, and admit Spanish Oak Red Cedar, Angelly and Venatica.	The same as in the preceding Class.	The same as in the preceding Class, and admit Other Continental White Oak Spanish Chesnut, Stringy Bark, Blue Gum and Pitch Pine.
and HOOKS	English, African, Adriatic, Italian, French, and Live Oak—East-India Teak—Morung Saul—Greenheart—Morra—Iron Bark—Mahogany of hard texture Cuba Sabicu Pencil Cedar.	The same as in the preceding class, and admit Spanish Oak Red Cedar, Angelly and Venatica.	The same as in the preceding Class.	The same as in the preceding Class, and admit Other Continental White Oak Spanish Chesnut, Stringy Bark, Blue Gum and Pitch Pine.
KNEES	English, African, Adriatic, Italian, French, and Live Oak—East-India Teak—Morung Saul—Greenheart—Morra—Iron Bark—Mahogany of hard texture Cuba Sabicu Pencil Cedar.	The same as in the preceding class, and admit Spanish Oak Red Cedar, Angelly and Venatica.	The same as in the preceding Class.	The same as in the preceding Class, and admit Other Continental White Oak Spanish Chesnut, Stringy Bark, Blue Gum and Pitch Pine.

* This Table applies to the Deadwood so far as regards the material to be used from the height of two-feet above the rabbet of the keel.
† Live Oak and Red Cedar admitted alternately in timbers of the frame for 10 A.

‡ If the First Futtocks run up above the Light Watermark, the use of Foreign White Oak is allowed for the 7 years' grade only.

MEM.—The word "English" includes Timber the growth of the United Kingdom.

Quality, to be used in the TIMBERING of SHIPS, as the same will be applicable for Ships to remain on the Character A.

SEVEN YEARS.	SIX YEARS.	FIVE YEARS.	FOUR YEARS.	PARTS OF THE FRAME OF A VESSEL.
The same as in the preceding Class, and admit Larch, Hackmatack, Tamarac, Juniper, Pitch Pine, Second-hand English or African Oak, or East-India Teak English Ash.	The same as in the preceding Class, and admit Cowdie American Rock Elm.	The same as in the preceding Class, and admit Baltic & Amer. Red Pine Foreign Ash Europ. & Amer. Grey Elm Black Birch Spruce Fir English Beech.	The same as in the preceding Class, and admit Hemlock.	FLOORS.
The same as in the preceding Class, and admit Larch—Hackmatack Tamarac—Juniper Pitch Pine Second-hand English or African Oak, or East-India Teak.	The same as in the preceding Class, and admit Cowdie English Ash American Rock Elm.	The same as in the preceding Class, and admit Baltic & Amer. Red Pine Foreign Ash European and American Grey Elm Black Birch Spruce Fir.	The same as in the preceding Class, and admit English Beech Hemlock.	
The same as in the preceding Class, and admit Other Continental White Oak, Spanish Chesnut Stringy Bark, Blue Gum North Amer. White Oak Amer. Sweet Chesnut Larch—Hackmatack Tamarac—Juniper Pitch Pine.	The same as in the preceding Class, and admit Second-hand English or African Oak, or East-India Teak Cowdie.	The same as in the preceding Class, and admit Baltic and American Red Pine English Ash American Rock Elm.	The same as in the preceding Class, and admit Foreign Ash European and American Grey Elm Black Birch Spruce Fir Hemlock.	1st FUTTOCKS.
The same as in the preceding Class, and admit Other Continental White Oak, Span. Ches. Stringy Bark, Blue Gum, N. Amer. White Oak, Amer. Sweet Chesnut, Larch, Hackmatack, Tamarac, Juniper Pitch Pine, Cowdie Baltic & Amer. Red Pine.	The same as in the preceding Class, and admit Second-hand English or African Oak, or East-India Teak.	The same as in the preceding Class, and admit English Ash American Rock Elm.	The same as in the preceding Class, and admit Foreign Ash European and American Grey Elm Black Birch Spruce Fir Yellow Pine Hemlock.	2d FUTTOCKS.
The same as in the preceding Class, and admit Cowdie Baltic and American Red Pine.	The same as in the preceding Class, and admit Second-hand English or African Oak, or East-India Teak American Rock Elm.	The same as in the preceding Class, and admit English Ash Foreign Ash.	The same as in the preceding Class, and admit European and American Grey Elm Black Birch Spruce Fir English Beech Yellow Pine.	3d FUTTOCKS and TOP TIMBERS.
The same as in the preceding Class, and admit Other Continental W. Oak Span. Ches., Stringy Bark Blue Gum, N. Amer. W. Oak, Amer. Sweet Ches. Larch, Hackmatack, Tamarac, Juniper, P. Pine.	The same as in the preceding Class, and admit Cowdie.	The same as in the preceding Class, and admit Second-hand English or African Oak, or East-India Teak Baltic and American Red Pine American Rock Elm.	The same as in the preceding Class, and admit English Ash—Foreign Ash European and American Grey Elm Black Birch Spruce Fir Yellow Pine.	MAIN and RIDER KEELSONS.
The same as in the preceding Class, and admit Other Continental White Oak, Spanish Chesnut, Stringy Bark, Blue Gum N. Amer. W. Oak, Amer. Sweet Chesnut, Larch Hackmatack, Tamarac Juniper, Pitch Pine.	The same as in the preceding Class, and admit Second-hand English or African Oak, or East-India Teak Cowdie.	The same as in the preceding Class, and admit Baltic and American Red Pine American Rock Elm.	The same as in the preceding Class, and admit English Ash Foreign Ash European and American Grey Elm Black Birch Spruce Fir Yellow Pine.	STEM and STERN POST.
The same as in the preceding Class, and admit North Amer. White Oak Amer. Sweet Chesnut Larch—Hackmatack Tamarac—Juniper Cowdie Baltic & Amer. Red Pine.	The same as in the preceding Class, and admit Second-hand English or African Oak, or East-India Teak.	The same as in the preceding Class, and admit English Ash Foreign Ash American Rock Elm European and American Grey Elm.	The same as in the preceding Class, and admit Black Birch Spruce Fir Yellow Pine Hemlock.	TRANSOMS KNIGHTHEADS HAWSE TIMBERS APRON and DEADWOOD.
The same as in the preceding Class, and admit North Amer. White Oak Amer. Sweet Chesnut Larch—Hackmatack Tamarac—Juniper Cowdie Baltic & Amer. Red Pine Spruce Fir.	The same as in the preceding Class, and admit Second-hand English or African Oak, or East-India Teak.	The same as in the preceding Class, and admit English Ash Foreign Ash American Rock Elm European and American Grey Elm.	The same as in the preceding Class, and admit Black Birch Yellow Pine Hemlock.	BEAMS and HOOKS.
				KNEES.

* Black Birch, Beech, American Rock Elm, and Cowdie allowed for Floors in Midships, to an extent not exceeding one-half the entire length of the keel in ships of the Seven Years' Grade.

** Black Birch and Spruce allowed for First Futtocks amidships, to the same extent in Ships of the Six Years' Grade.

White Cedar allowed for Third Futtocks and Toptimbers in ships of the Seven Years' Grade.

No. 2.—A TABLE exhibiting the different Descriptions of TIMBER, of good
applicable to the several Terms of Years appointed

PARTS OF THE OUTSIDE OF A VESSEL.	TWELVE YEARS.	TEN YEARS.	NINE YEARS.	EIGHT YEARS.
KEEL to the 1st FUTTOCK HEADS.....	English, African, and Live Oak, East-India Teak, Morung Saul, Greenheart, Morra, Iron Bark, Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Adriatic, Italian, Spanish, and French Oak, Red Cedar, Angelly and Ve- natica, other Continental White Oak, Spanish Chesnut, Stringy Bark and Blue Gum, North American White Oak, American Sweet Ches- nut, Larch, Hackma- tack, Tamarac, Juniper, Pitch Pine, § American Rock Elm, § European and American Grey Elm, § English Beech.	The same as in the pre- ceding Class, and admit Cowdie English Ash Foreign Ash Black Birch.	The same as in the pre- ceding Class, and admit Baltic and American Red Pine.	The same as in the pre- ceding Class.
1st FUTTOCK HEADS to LIGHT WATER MARK	English, African, and Live Oak, East-India Teak, Morung Saul, Greenheart, Morra, Iron Bark, Mahogany of hard texture, Cuba Sabicu, Pencil Cedar, Adriatic, Italian, Spanish, and French Oak, Red Cedar, Angelly and Ve- natica, other Continental White Oak, Spanish Chesnut, Stringy Bark and Blue Gum, Pitch Pine.	The same as in the pre- ceding Class, and admit North American White Oak American Sweet Chesnut Larch Hackmatack Tamarac Juniper.	The same as in the pre- ceding Class, and admit Cowdie Baltic and American Red Pine.	The same as in the pre- ceding Class, and admit American Rock Elm European and American Grey Elm English Beech.
LIGHT WATER MARK to WALES	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark Red Cedar Angelly and Venatica.	The same as in the pre- ceding Class, and admit Mahogany of hard tex- ture—Cuba Sabicu Pencil Cedar Adriatic, Italian, Spanish, and French Oak.	The same as in the pre- ceding Class, and admit Other Continental White Oak Spanish Chesnut Stringy Bark, Blue Gum and Pitch Pine.	The same as in the pre- ceding Class, and admit N. American White Oak Amer. Sweet Chesnut Larch—Hackmatack Tamarac Juniper Cowdie Baltic & Amer. Red Pine.
WALES BLACKSTRAKES TOPSIDES..... and SHEERSTRAKES	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the pre- ceding Class, and admit Mahogany of hard tex- ture Adriatic, Italian, and French Oak Cuba Sabicu Pencil Cedar Red Cedar Angelly and Venatica.	The same as in the pre- ceding Class, and admit Spanish Oak Pitch Pine.	The same as in the pre- ceding Class, and admit Other Continental White Oak Spanish Chesnut Stringy Bark, and Blue Gum
UPPER DECK WATERWAYS SPIRKETTING and PLANKSHEERS.	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra—Iron Bark.	The same as in the pre- ceding Class, and admit Adriatic, Italian, Spanish, and French Oak Red Cedar Angelly and Venatica Larch—Hackmatack Tamarac—Cuba Sabicu Juniper Pitch Pine—Pencil Cedar Cowdie Baltic & Amer. Red Pine. Mahogany of hard tex- ture.	The same as in the pre- ceding Class, and admit Other Continental White Oak Spanish Chesnut Stringy Bark and Blue Gum.	The same as in the pre- ceding Class.

§ The use of Elm and Beech, in Ships above the EIGHT YEARS' grade, to be restricted to a height from the lower part of the main Keel, of one-third of the internal depth of the Ship measured, in midships, from the top of the Limber Strake to the top of the Upper Deck Beams.

Quality, to be used in the OUTSIDE PLANKING of SHIPS, as the same will be for Ships to remain on the Character A.

SEVEN YEARS.	SIX YEARS.	FIVE YEARS.	FOUR YEARS.	PARTS OF THE OUTSIDE OF A VESSEL.
The same as in the preceding Class.	The same as in the preceding Class, and admit Spruce Fir Yellow Pine.	The same as in the preceding Class.	The same as in the preceding Class, and admit Hemlock.	KEEL to the 1st FUTTOCK HEADS.
The same as in the preceding Class, and admit English Ash Foreign Ash Black Birch.	The same as in the preceding Class, and admit Spruce Fir	The same as in the preceding Class, and admit Yellow Pine.	The same as in the preceding Class, and admit Hemlock.	1st FUTTOCK HEADS to LIGHT WATER MARK.
The same as in the preceding Class.	The same as in the preceding Class, and admit American Rock Elm.	The same as in the preceding Class, and admit European and American Grey Elm Yellow Pine Spruce Fir.	The same as in the preceding Class, and admit English Ash Foreign Ash Black Birch English Beech Hemlock.	LIGHT WATER MARK to WALES.
The same as in the preceding Class, and admit North Amer. White Oak American Sweet Chesnut Larch - Hackmatack Tamarac Juniper Cowdie Baltic & Amer. Red Pine.	The same as in the preceding Class.	The same as in the preceding Class, and admit American Rock Elm Yellow Pine.	The same as in the preceding Class, and admit European and American Grey Elm Black Birch Spruce Fir Hemlock.	WALES BLACKSTRAKES TOPSIDES and SHEERSTRAKES
The same as in the preceding Class, and admit North American White Oak American Sweet Chesnut.	The same as in the preceding Class.	The same as in the preceding Class, and admit Second-hand English or African Oak East-India Teak American Rock Elm †† Yellow Pine.	The same as in the preceding Class, and admit European and American Grey Elm Black Birch Spruce Fir Hemlock.	UPPER DECK WATERWAYS SPIRKETTING and PLANKSHEERS.

†† Yellow Pine allowed for Waterways of Upper Deck in Ships of the SEVEN YEARS' grade, if properly fastened, as prescribed in Table B, and provided the Beams are well secured, independently of the Waterways.

MEM.—The word "English," includes Timber the growth of the United Kingdom.

No. 3.—A TABLE exhibiting the different Descriptions of TIMBER, of good applicable to the several Terms of Years

INSIDE PLANK.	TWELVE YEARS.	TEN YEARS.	NINE YEARS.	EIGHT YEARS.
LIMBER STRAKES, BILGE STRAKES, and CEILING BETWEEN THEM.	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark Mahogany of hard texture Cuba Sabicu Pencil Cedar Adriatic, Italian, Spanish, and French Oak Red Cedar Angelly and Venatica.	The same as in the preceding Class, and admit Other Continental White Oak Spanish Chesnut Stringy Bark and Blue Gum.	The same as in the preceding Class, and admit North American White Oak American Sweet Chesnut Pitch Pine.	The same as in the preceding Class.
CEILING, LOWER HOLD, and BETWEEN DECKS.	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark Mahogany of hard texture Cuba Sabicu Pencil Cedar Adriatic, Italian, Spanish, and French Oak Red Cedar Angelly and Venatica.	The same as in the preceding Class, and admit Other Continental White Oak Spanish Chesnut Stringy Bark and Blue Gum.	The same as in the preceding Class, and admit North American White Oak American Sweet Chesnut Pitch Pine.	The same as in the preceding Class, and admit Larch Hackmatack Tamarac Juniper Cowdie Baltic and American Red Pine.
SHELF PIECES CLAMPS and LOWER DECK WATERWAY.	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark Mahogany of hard texture Cuba Sabicu Pencil Cedar Adriatic, Italian, Spanish, and French Oak Red Cedar Angelly and Venatica.	The same as in the preceding Class, and admit Other Continental White Oak Spanish Chesnut Stringy Bark and Blue Gum.	The same as in the preceding Class, and admit North American White Oak American Sweet Chesnut Pitch Pine.	The same as in the preceding Class, and admit Larch Hackmatack Tamarac Juniper Cowdie Baltic and American Red Pine.
RUDDER and WINDLASS MAIN PIECES.	English } Oak African } Live } East-India Teak Morung Saul Greenheart Morra Iron Bark.	The same as in the preceding Class, and admit Mahogany of hard texture Adriatic, Italian, and French Oak Cuba Sabicu Pencil Cedar.	The same as in the preceding Class, and admit Spanish Oak.	The same as in the preceding Class.

|| The materials marked thus || under the head of "Rudders and Windlass" allowed in ships of 300 Tons and under only.

Quality, to be used in the INSIDE PLANKING of SHIPS, as the same will be for Ships to remain on the Character A.

SEVEN YEARS.	SIX YEARS.	FIVE YEARS.	FOUR YEARS.	INSIDE PLANK.
The same as in the preceding Class, and admit Larch Hackmatack Tamarac Juniper Cowdie Baltic and American Red Pine American Rock Elm.	The same as in the preceding Class.	The same as in the preceding Class, and admit Second hand English or African Oak or East-India Teak English Ash Foreign Ash European and American Grey Elm Black Birch Spruce Fir English Beech Yellow Pine.	The same as in the preceding Class, and admit Hemlock.	LIMBER STRAKES, BILGE STRAKES, and CEILING BETWEEN THEM.
The same as in the preceding Class.	The same as in the preceding Class, and admit American Rock Elm.	The same as in the preceding Class, and admit Second-hand English or African Oak or East-India Teak English Ash Foreign Ash European and American Grey Elm Black Birch Spruce Fir English Beech Yellow Pine.	The same as in the preceding Class, and admit Hemlock.	CEILING, LOWER HOLD, and BETWEEN DECK.
The same as in the preceding Class.	The same as in the preceding Class, and admit American Rock Elm.	The same as in the preceding Class, and admit Second-hand English or African Oak or Teak and Yellow Pine Spruce Fir.	The same as in the preceding Class, and admit English Ash Foreign Ash European and American Grey Elm Black Birch English Beech Hemlock.	SHELF PIECES CLAMPS and LOWER DECK WATERWAY.
The same as in the preceding Class, and admit Other Continental White Oak, Spanish Chesnut Stringy Bark, Blue Gum North American White Oak and American Sweet Chesnut ||Larch, ||Hackmatack ||Tamarac, ||Juniper, ||Pitch Pine.	The same as in the preceding Class.	The same as in the preceding Classes, and admit Second-hand English or African Oak or East-India Teak Baltic and American Red Pine ||English Ash.	The same as in the preceding Classes, and admit American Rock Elm ||Black Birch ||Spruce Fir ||English Beech.	RUDDER and WINDLASS MAIN PIECES.

MEM.—The word "English" includes Timber the growth of the United Kingdom.

FORM OF THE REPORT OF ORIGINAL SURVEY.

No. — Survey held at — Date — 18 — on the — Master —
 Tonnage { Old — Built at — When built — Launched —
 { New —
 By whom built — Owners — Port belonging to — Destined Voyage —
 If Surveyed while building, Afloat, or in Dry Dock —

Length aloft	Feet.		Inches.		Extreme Breadth outside	Feet.		Inches.		Depth of Hold	Feet.		Inches.	

SCANTLINGS OF TIMBER.

THICKNESS OF PLANK.

	IN SHIP.		REQUIRED PER RULE.		OUTSIDE.		INSIDE.	
	Sided.	Moulded.	Sided.	Moulded.	In Ship.	Required per Rule.	In Ship.	Required per Rule.
Timber and Space					Garboard		Limber Strakes	
Floors					Strakes		Bilge Planks	
1st Foothooks					Garboard to Bilge		Ceiling in Flat	
2d Ditto					Bilge Planks		Ditto Bilge to Clamp	
3d Ditto					Bilge to Wales		Hold Beam Clamps..	
Top Timbers					Wales		Deck Beam Ditto ...	
Deck Beams No. { Average					Topsides		Ceiling 'twixt Decks	
Deck Beams, length amidships { Space					Sheerstrakes		Hold Beam Shelves ..	
Hold Beams No. { Average					Planksheers		Deck Beam ditto.....	
Hold Beams, length amidships { Space					Waterways—			
Keel					Upper Deck			
Scarphs of Ditto.....					Lower Deck			
Keelsons.....					Do. faying surface against			
Scarphs of Ditto					Timbers ...			
					Upper Deck			

SIZE OF BOLTS IN FASTENINGS, DISTINGUISHING WHETHER COPPER, YELLOW METAL, OR IRON; ALSO OF TREENAILS.

	Copper or Y.M. in Ship.	Iron in Ship.	Inches required per Rule		Copper or Y.M. in Ship.	Iron in Ship.	Inches required per Rule
Heel-Knee and Deadwood abaft				Thickstuff over Double Floors			
Scarphs of Keel, No.				Butt End Bolts			
Keelson Bolts through Keel at each Floor				Pinles of the Rudder			
Bolts through Heels of Timbers against Deadwood...				Hold Beam { Waterway			
Transoms and throats of Hooks.....				Bolts in { Knees			
Arms of Hooks				Deck Beam { Shelf or Clamp			
Through Bilge and Limber Strakes				Bolts in { Waterway			
				Nails or Bolts in Flat of Deck			
				Treenails..... Inches			

TIMBERING.—The Space between the Floor Timbers and Lower Foothooks is — Inches.

The Space between the Top Timbers is — Inches.

The Floors consist of — The First Foothooks of —

The Second Foothooks of — The Third Foothooks and Top Timbers of —

The Shifts of the first and second Foothooks are not less than —

[N.B. When less than prescribed by the Rule, state how many.]

The rest of the Shifts of the Frame are —

The Frame is — squared from the first Foothook Heads upwards, and — free from sap, and from thence downwards the Frame is —

The — Frames are — bolted together to the Gunwale.

[N.B. If not, state how bolted.]

The Butts of the Timbers are — close together; their thickness not less than — of the entire moulding at that place.

The frame is — chocked with — Butt at each end of the chock. The Main piece of Rudder is — Of Windlass is —

The Keel is — The Main Keelson is — and — free from all defects.
 The Stem and Stern Post of — The Transoms, Knight Heads, Hawse Timbers,
 and Aprons, of — Deadwood, of — and are — free from all defects.

The Deck and Hold Beams of — The Breasthooks of — The Knees of —

PLANKING OUTSIDE.—From the Keel to the Height defined in Note to Table A, or — to the

First Foothook Heads the Plank is —

From the above-named height to the Light Water Mark —

From the Light Water Mark to the Wales —

The Wales and Blackstrakes are — The Topsides and Sheerstrakes —

The Spirketting and Planksheers — The Waterways { Upper Deck —
 Lower Deck —

The Decks — State of —

The Shifts of the Planking are not less than — feet — inches. [*N.B.—If less than pre-
 scribed by the Rule, state whether general or partial, and if partial, in what part of the Ship.*]

The Planking is wrought — between, and without step-butting.

PLANKING INSIDE.—The Limber-strakes and Bilge-strakes are —

The Ceiling, Lower Hold, and between Decks — Shelf Pieces and Clamps —

FASTENINGS.—To Hold Beams —

Deck Beams —

Number of Breasthooks — Pointers — Crutches —

Butts End Bolts are of — in the Bottom — Bolts in each Butt End —
 through and clenched.

Bilge and Limber Strakes — bolted through and clenched. Treenails of — How
 made —

Thickstuff over Double Floors — bolted through and clenched. General Quality
 of Workmanship —

We certify that the above is a correct description of the several particulars therein given.

Builder's Signature — *Surveyor's Signature* —

Her Masts, Yards, &c. are in — condition, and sufficient in size and length.

She has SAILS.

CABLES, &c.

ANCHORS,

No.		Fathoms.	Inches.	and their Weights.
	Fore Sails,	Chain		No. Weight.
	Fore Top Sails,	Hempen Stream Cable		Bower
	Fore Topmast Stay Sails,	Hawser		Stream
	Main Sails,	Towlines.....		
	Main Top Sails,	Warp		Kedge.....
	and	All of — quality.		

Her Standing and Running Rigging — sufficient in size and — in quality.

She has — Long Boat and — The present state of the Windlass is —

Capstan — Rudder — Pumps —

General Remarks and Statement and Date of Repairs, if any.

Dates of Surveys { 1st. When the Frame is completed —
 held while building, { 2nd. When the Beams are put in, &c. —
 as per Section 35. { 3rd. When completed, and before the plank be painted or payed —
 Present condition of Caulking of Bottom, — Deck, — and Waterways —
 If Sheathed, Doubled, Felted, or Coppered — When last done —
 I am of opinion this Vessel should be classed —
 The Amount of the Fee £ : : is received by me,
 Special £ : :
 Certificate £ : :

Committee's Minute — 18—

Character assigned —

No. — Survey held at — Date — 18— on the — Master —
 Tonnage—Gross — Engine Room — Register — Built at —
 When built — By whom built — Owners — Port belonging to —
 Destined Voyage — If Surveyed Afloat or in Dry Dock —

[illegible]

Transoms, material ——— or, if none, in what manner compensated for.

Knight-heads „ }
Hawse Timbers „ } are they free from defects?

Bulkheads, No. ——— Thickness of ———

„ how secured to the sides of the ship ———

„ size of vertical angle iron and their distance apart ———

The Frames or Ribs extend in one length from ——— to ——— rivetted through plates with (in.) rivets, about () apart.

The reverse angle irons on the floors extend in one length across the middle line from ——— to ———

„ „ on the frames „ „ „ from ——— to ———

Keelson, how are the various lengths of plates or angle irons connected? ———

Plates, Garboard, double or single rivetted to keel and at upper edge, with rivets (ins.) diameter, averaging (in.) from centre to centre of rivet.

„ edges from Garboards to upper part of bilge, worked carvel with a lining piece (in.) thick, or clencher, double or single rivetted; rivets (in.) diameter, averaging (ins.) from centre to centre of rivets.

„ butts from keel to turn of bilge, worked carvel with a lining piece () thick double or single rivetted; rivets (in.) diameter, averaging (in.) from centre to centre of rivets. Do the lining pieces lap over and rivet through the lands of the strake below? ———

„ edges from bilge to planksheer, worked carvel with a lining piece () thick, double or single rivetted; rivets (in.) diameter, averaging (ins.) from centre to centre of rivets. Do the lining pieces lap over and rivet through the lands of the strake below?

„ butts from bilge to planksheers, worked carvel with a lining piece () thick, or clencher, double or single rivetted; rivets (in.) diameter, averaging (ins.) from centre to centre of rivets. Breadth of laps in double rivetting () Breadth of laps in single rivetting ().

Planksheer, how secured to the plating of the sides { Explain by a sketch, }
Waterway „ „ planksheer and to the beams { if necessary. }

Side trussing ——— breadth and thickness of plates ——— how secured? ———

Deck trussing ——— „ „ „ ——— „ „ ———

Deck Beams, how secured to the side ———

Hold or Lower Deck „ ———

Paddle „ „ ———

No. of breasthooks ——— crutches ——— how are pointers compensated?

What description of iron is used for the angle iron and plate iron in the vessel?

————— Builder's Signature.

WORKMANSHIP.—Are the lands or laps of the clench work in all cases in breadth at least five times the diameter of the rivets in double rivetted edges and butts, and at least three times the diameter of the rivets where single rivetting is admitted? ———

Do the edges of the carvel work and of the butts lay close together throughout their length without requiring any making good of deficiencies? ———

Do the fillings between the ribs and plates fill in solid with single pieces, or are they in short lengths of various thicknesses? ———

Do the holes for rivetting plate to frames, lining pieces, or plate to plate, &c., conform well to each other? ——— and are the rivet holes well and sufficiently counter sunk in the outer plate? ———

Are there any rivets which either break into or have been put through the seams or butts of the plating? ———

Her Masts, Yards, &c., are in ——— condition, and sufficient in size and length.

She has SAILS.		CABLES, &c.		ANCHORS, and their weights.	
No.			Fathoms. Inches.	No.	Weight.
	Fore Sails,	Chain			Bower,.....
	Fore Top Sails,	Hempen Stream Cable			Stream,
	Fore Topmast Stay Sails,	Hawser			Kedge,.....
	Main Sails,	Towlines.....			
	Main Top Sails	Warp			
	and	All of — quality.			

Her Standing and Running Rigging ——— sufficient in size, and ——— in quality.

She has ——— Long Boat and ———

The present state of the Windlass is ——— Capstan ——— and Rudder ———
Pumps ———

GENERAL REMARKS.

Statement and date of repairs; extent of corrosion (if any) both internally and externally; and condition of rivets.

Dates of Surveys held while building, as per Section 17.	1st. On the several parts of the frame, when in place, and before the plating was wrought ———
	2nd. On the plating during the progress of rivetting ———
	3rd. When the beams were in and fastened, and before the decks were laid ———
	4th. When the ship was complete, and before the plating was finally coated ———
	5th. After the ship was launched ———

In what manner are the surfaces preserved from oxidation? ———

I am of opinion this vessel should be classed ———

The amount of the Fee .. £ : : is received by me,

Special £ : :

Certificate (if required) £ : :

Committee's Minute ——— 18—

Character assigned ———

No. 6.

FORM OF REPORT OF ANNUAL SURVEY.

No. — Survey held at — Date — 18 — on the —
 Master — Tonnage — Built at — When built — By whom
 built — Owners — Port belonging to — Destined
 Voyage — If Surveyed Afloat or in Dry Dock —

Last Survey, No. — Port of — Classed —

The present condition of the

Decks	Plank (Bottom) & Counter.	Windlass and Capstan
Waterways	Treenails.....	Pumps.....
Comings	Breasthooks and Stemson...	Boats
Upper Deck Beams and	Transoms, Pointers, and	Masts, Yards, &c.
Fastenings	Crutches.....	Sails.....
Lower Deck Beams and	Timbers of the Frame	Anchor, No. of.....
Fastenings	Keelsons.....	Cables.....
Planksheers	Clamps and Shelves.....	Hawsers and Warps
Sheerstrakes	Ceiling.....	Standing & Running Rigging
Topsides	Rudder	Caulking of Bottom, Deck,
Wales	Copper, when put on	and Waterways

General Observations and Opinion,

Committee's Minute — 18

Character assigned —

Certificate (if required)

No. 7.

FORM OF CERTIFICATE OF CHARACTER.

Lloyd's Register of British and Foreign Shipping.

ESTABLISHED 1834.

No. —



London,

18

No. 2, White Lion Court, Cornhill.

These are to Certify, That the — of —
 — Master, — Tons, bound to —, has been
 Surveyed at — by the Surveyors to this Society, and reported to be, on the

and that she has been **CLASSED** and entered in the **REGISTER BOOK** of this
 Society with the character —

Charge

Witness my hand,

Chairman.

Secretary.

FORM OF CERTIFICATE FOR VESSELS NAVIGATED BY STEAM.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

Certificate for Vessels navigated by Steam.

_____ Day of _____ 18

_____ certify that the whole of the Boilers and machinery of the
 Steam Vessel _____ belonging to _____, whereof _____
 is Master, _____ Tons, have been carefully inspected and examined by _____
 at _____, and _____ find the same to be at this time in good order
 and safe working condition,

Witness my hand,

Manufacturing Engineer.

The following is a true Account of the Particulars of the machinery of the
 Steam Vessel above named :

ENGINES.

Number
 Diameter of Cylinder
 Length of Stroke
 No. per Minute
 Estimated Power
 Diameter of Paddle-wheels
 Length of Paddles
 Breadth of Paddles
 No. of Paddles
 On what motion.....
 No. of revolutions per minute ..
 Size of the holding-down bolts ..
 Condition of ditto
 Maker of the Engines
 Age of the Engines
 When were they last taken out
 Present condition of the Engines
 Can injection water be taken
 from the Bilge in the event of
 a serious Leak

FUEL.

Where stowed
 If in contact with boiler
 If not, what space between Coal
 Boxes and Boiler
 For what quantity is room pro-
 vided
 If liable to get wetted.....

BOILERS.

Whether iron or copper.....
 Working pressure
 Can it be increased without going
 into the Boiler
 What are the means of changing
 the water while the Boilers are
 at work
 Maker of the Boilers.....
 Age of the Boilers.....
 When were they last taken out
 Present condition

Number of feed pumps
 How attached.....

What clear space upon the top-
 side of the boiler from wood-
 work
 Do. at the end from ditto.....
 Do. round the chimney from do.

PUMPS.

No. of hand pumps
 Can they be worked by the En-
 gine
 If any attached to engine, their
 purpose and size.....
 No. of force-pumps
 No. of branches and hose of
 sufficient length to reach to
 every part of the vessel.....

Manufacturing Engineer.

KEY TO THE REGISTER

The Stores of Vessels are designated by the Figures 1 and 2 :—

1. Signifying that the Vessel is well and sufficiently found.
2. That she is deficient in either quantity or quality.

Vessels RESTORED to, or CONTINUED on the Character A, will have the number of Years for which they are restored or continued inserted separately under the number originally assigned, in the tenth column; where the term in the latter case has expired, the Letter "C" only will be placed before the figure denoting the number of Years for which Continuation had been allowed.

Vessels marked "S.S." with a date, *in red*, denote that they have been surveyed at that time in accordance with the *second Clause* of the Rules, Sec. 60.

The mark [*B. S.*] denotes that the vessel is classed subject to *Biennial Survey*. The mark [*Expt. B.S.*] denotes that the vessel is built experimentally, and classed subject to *Biennial Survey*. See Section 31.

The mark (*t. s.*) with a date, denotes that the vessel has been submitted to Triennial Survey, as prescribed by Rules for Iron Ships, Section 17.

British North-American built vessels, opened as required by the Rules, Sec. 63, will be marked in the Register "S.S." with the number of years that may elapse before they are again opened, and the year of survey; thus—"S.S.59—3Yrs—"

When the term "almost Rebuilt" is used, it implies large repairs, but that the Rules for Restoration have not been fully complied with.

The figures in the Tenth Column, to vessels in the *Æ* Class, imply that they were originally classed A, for that number of Years. The cipher 0 in this Column indicates that (from inferior construction) there were no "number of Years first assigned."

Ships built under Special Survey will be shown by a Cross, thus ✕.

In the Second Column.

Bk.....Barque	K .....Ketch	Sr .....Schooner
Bg.....Brig	Lr .....Lugger	St .....Schoot
Bn.....Brigantine	Pol.....Polacre	Stm.....Steamer
Cr.....Cutter	S.....Ship	Sw .....Snow
Dr.....Dogger	Scw .....Screw	Yt .....Yacht.
G .....Galliot	Sk .....Smack	
H .....Hoy	Sp .....Sloop	

pt.—part—s.—sheathed—d.—doubled—C.—coppered—I. B.—
 Iron Bolts—c.f.—copper fastened—M.—sheathed with Marine Metal
 —YM.—sheathed with Yellow Metal—G. Iron—sheathed with
 Galvanized Iron—Z.—sheathed with Zinc—F.—Felt.
 C.lm.—Coppered to light water or ballast mark.
 C.T.—Copper Bolts substituted for Treenails.

In the Third Column.

Sailing Vessels thus distinguished, "A P. 30 H." denote that they are Fitted with Auxiliary Steam Power, equal to 30 Horses.

In the Fourth Column.

H.P.—Horse Power. M.C.—Machinery certified. Where two Tonnages are given, the upper one is the Measurement by the old Act, the under one is that by the new Act, which commenced the 1st January, 1836.

KEY TO THE REGISTER.

In the Fifth Column.

The Timber of which vessels are built is marked by the following *Italic Letters* :

<i>A</i> Ash	<i>Hm</i> Hemlock	<i>RP</i> Red Pine
<i>BB</i> Black Birch	<i>J</i> Juniper	<i>YP</i> Yellow Pine
<i>Bh</i> Beech	<i>L</i> Locust	<i>S</i> Spruce
<i>C</i> Cedar	<i>Lh</i> Larch	<i>Sb</i> Sabicu
<i>Chs</i> Chesnut	<i>LO</i> Live Oak	<i>Sl</i> Saul
<i>E</i> Elm	<i>M</i> Mahogany	<i>T</i> Teak
<i>F</i> Fir	<i>O</i> Oak	<i>Tam</i> Tamarac
<i>G</i> Gum	<i>P</i> Pine	<i>WH</i> Witch Hazel
<i>Ght</i> Greenheart	<i>PP</i> Pitch Pine	<i>WO</i> White Oak
<i>Hk</i> Hackmatack		

Cl.—Clinker—len.—lengthened—Irp.—large repairs—Srprs—Some
 repairs—Drp.—Damage Repaired—nd.—New Deck—nTSds—New
 Top-Sides—w.s.—Wales sheathed—nw.—New Wales—NB.—New
 Bottom—nKl.—New Keel—plk.—Plank—nKlsn—New Keelson
 —alm. rb.—almost rebuilt—pt O.M.—part old Timbers or Plank—
 Rest.—Restored—Cont.—Continued.—Blk Hds.—Bulk Heads.—
 Mat.—Materials.

*In the Eighth Column:—U.S. for American Property.**In the Ninth Column (Surveying Ports):*

Abn.. Aberdeen	Drt.. Dartmouth	Lon.. London	St. I.. St. Ives
Abs.. Aberystwith	Dub.. Dublin	Ldy.. Londonderry	St. J.. St. John
All.. Alloa	Dum.. Dumfries	Lwt.. Lowestoft	Ser.. Scarborough
Arb.. Arbroath	Dun.. Dundee	Lyn.. Lynn	Sil.. Scilly
Bng.. Bangor	Dgr.. Dungarvon	Mar.. Margate	Shl.. { Shields, N.
Bnf.. Banff	Exr.. Exeter	Mpt.. Maryport	Shl.. { Shields, S.
Bar.. Barmouth	Fal.. Falmouth	Mil.. Milford	Shm.. Shoreham
Bpl.. Barnstaple	Foy.. Fowey	Mch.. Miramichi	Sli.. Sligo
Bms.. Beaumaris	Gal.. Galway	Mth.. Monmouth	Sou.. Southampton
Bel.. Belfast	Glr.. Gloucester	Mtr.. Montrose	Stk.. Stockton
Bwk.. Berwick	Gmb.. Grangemouth	N Sc.. Nova Scotia	Str.. Stranraer
Bid.. Bideford	Goo.. Goole	Nwc.. Newcastle	Sld.. Sunderland
Blg.. Belgium	Gns.. Guernsey	Nhv.. Newhaven	Sws.. Swansea
Bly.. Blyth	Hgt.. Harrington	Npt.. Newport	Tgn.. Teignmouth
Bos.. Boston	Hpl.. Hartlepool	Nry.. Newry	Tps.. Topsham
Bdg.. Bridgewater	Har.. Harwich	Ork.. Orkneys	Tqy.. Torquay
Bpt.. Bridport	Hol.. Holland	P. E. I. { Prince Ed.	Wtf.. Waterford
Brs.. Bristol	Hul.. Hull	ward Island	Wls.. Wells
Car.. Cardigan	Inv.. Inverness	Pad.. Padstow	Wex.. Wexford
Cff.. Cardiff	Ips.. Ipswich	Pnz.. Penzance	Wey.. Weymouth
Chp.. Chepstow	I.M.. Isle of Man	Pet.. Peterhead	Wtb.. Whitby
Chs.. Chester	Jer.. Jersey	Ply.. Plymouth	Whn.. Whitehaven
Cly.. { Clyde	Lan.. Lancaster	Poo.. Poole	Wrk.. Workington
{ Glasgow, &c.	Lth.. Leith	Pts.. Portsmouth	Yar.. Yarmouth
Crk.. Cork	Lim.. Limerick	Pmd.. Portmadoc	Ygh.. Youghal
Cws.. Cowes	Liv.. Liverpool	Ram.. Ramsgate	
Dov.. Dover	Lly.. Llanelly	Qbc... Quebec	

In the Eleventh Column.

The Figures under the Character denote the date when the Vessel was last surveyed.

LLOYD'S REGISTER OF BRITISH AND
FOREIGN SHIPPING.

SHIPS CLASSED A IN RED, AND Æ, INCLUDING
THOSE WITH THE ASTERISK.

THE Rules, Sections 60 and 61, requiring that Ships classed A in red, or Æ (including those with the Asterisk), shall be surveyed annually, or on their return from every foreign voyage;—

NOTICE is hereby given, that in accordance with the above Rules, and in pursuance of a Resolution passed this day by the Committee, the Characters of Ships classed A in red, or Æ (including those with the Asterisk), which shall not have been surveyed since the year 1860, will be omitted in reprinting the Register Book (in June next), for the year 1862—63.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, White Lion Court, Cornhill, London, E.C.,

7th November, 1861.

N.B.—In the case of Ships which it shall be made to appear, by letter addressed to the Secretary, have not been in any port in the United Kingdom since 1860, the above Resolution will not be applied.

LLOYD'S REGISTER OF BRITISH AND
FOREIGN SHIPPING.

SHIPS CLASSED A
WHOSE CHARACTERS EXPIRE on 31st DECEMBER, 1861.

NOTICE is hereby given that with a view to prevent inconvenience to owners of Vessels which may be absent from the United Kingdom, a Resolution has this day been passed by the Committee :— That Ships classed A for a term of years, and which term will expire at the end of the year 1861, shall be allowed to retain the Character A until the reprinting of the Register Book in June next; subject, however, to their being placed under survey at the first convenient opportunity.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, White Lion Court, Cornhill, London, E.C.,

7th November, 1861.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

ANCHORS AND CABLES.

NOTICE is hereby given that the Committee of this Society have for some time had the subject of their recommendations contained in Table No. 22, attached to the Rules, regarding Anchors and Cables, under their consideration, and in reference thereto they have passed the following Resolutions, viz.:—

- 1.—That the Table No. 22 be amended by defining the minimum weight for Anchors of approved make, whether ordinary or patent, thus discontinuing any distinction in favour of (so called) Patent Anchors.
- 2.—That in future one weight only for Anchors be recognized, and that the lighter weight, or that hitherto required for Anchors with wood stocks, be adopted in framing the scale.
- 3.—That it be made a Rule that two of the Bower Anchors be of at least the weight set forth in the Table, and that then the third Anchor may be lighter; the reduction in weight, however, not to exceed 15 per cent.
- 4.—That the Stream Anchors be one-third the weight of the Bowers, the first Kedge Anchor one-half the weight of the Stream Anchors, and the second Kedge one-half the weight of the first Kedge Anchor.
- 5.—That all Anchors be subjected to a proof strain equal to that shown in the Table No. 22.
- 6.—That the recognition of the test for Chain Cables known as the "Ordinary" or "Merchant" proof be discontinued; and that all new ships be required to be supplied with Chains which have been submitted to a proof strain equal to that known as the "Admiralty test," and of the sizes shown in the Table No. 22. And that after the 1st January, 1863, all chains supplied to ships be subject to the same regulations.
- 7.—Parties desirous of using or supplying Chains of smaller size, and who are willing to subject them to a greater strain than is set forth above, may submit their propositions to the Committee.
- 8.—That Chains already made which will not bear the Admiralty test, may be admitted in lieu of lighter chains, according to the proof to which they have been subjected. For example—a 2 in. chain which has been proved to the extent required by the Admiralty test for a 1½ in. chain may be admitted to serve as a 1½ in. chain.

The above Resolutions, so far as they relate to *the weight of Anchors*, will come into force, as a rule, at the end of six months from the present date.

The regulations in regard to the testing of Anchors and Chain Cables will not be enforced until 1st January, 1863.

Copies of the Table No. 22, amended as above, may be had on application at this office, or from the Surveyors at the several outposts.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, White Lion Court, Cornhill, London, E.C.,
23rd January & 27th March, 1862.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

SHIPS CLASSED A 1 IN RED.

NOTICE.

IN pursuance of Resolutions this day passed by the Committee, the Rule, Sec. 60, as applicable to ships built in the British North American Colonies, has been amended as follows, viz. :—

“ All British North American built Ships, and all Fir Ships seeking to be classed with the character A in red, must be surveyed in conformity with the *second* clause of the Rule, Sec. 60 ; and, in addition thereto, such Ships of 400 tons and upwards, must be *doubled* from the keel to above the first strake of lower deck spir-ketting.”

In connection with the above regulation the Rule, Sec. 68, has also been altered as follows :—

“ If the doubling be applied diagonally, it will be allowed to be of the following thicknesses, viz. :—

In ships under 500 tons	1½ inch.
„ 500 tons and under 1000 tons	2 „
„ 1000 tons and upwards	2½ „

“ No Ship hereafter doubled shall be entitled to the character A, or A in red, unless at the time of doubling it be ascertained, by removing plank, or cutting out listings, all fore and aft in either case, that the frame is capable of securely retaining the fastenings.”

By Order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, White Lion Court, Cornhill, London, E.C.,

20th March, 1862.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

RULES AND REGULATIONS.

NOTICE is hereby given that in pursuance of Resolutions passed this day by the Committee, the following amendments have been made in the Rules, viz. :—

“ The garboard strakes, main sheerstrakes, and hollow or flat keel plates of Iron Ships are not to be less in breadth than as under, viz. :—

In ships under 500 tons	2ft.
„ 500 and under 1000 tons	2ft. 6in.
„ 1000 and upwards	3ft.

“ Sec. 46.—Not less than two-thirds of the treenails are in future to be driven through the inside planking, clamps, &c., instead of one-half, as at present required, and the proportion is to be preserved in all parts of the ship.

“ In cases where Iron beams are fitted in wood Ships, the Beams of each deck are to be $\frac{1}{8}$ of an inch thicker than is required by the rules for Ships built of Iron, in consideration of the greater space between them; and the depth of lower deck or hold Beams is to be increased one inch. The spaces between beams of the several decks not to exceed the spaces at present allowed for wood Ships, as per Rule, Sec. 41. Each tier of beams must have stringer plates rivetted on their ends, and tie-plates fore and aft, on each side of the hatchways, in accordance with the Rules for Iron Ships, Sec. 15, and to be of the dimensions required in Table G, or of strength equal thereto. In other respects parties are to submit their plans for attaching the Beams to the Ship's sides, for the Committee's approval.”

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

2, White Lion Court, Cornhill, London, E.C.,
17th April, 1862.

CIRCULAR TO SURVEYORS.

LLOYD'S REGISTER OF BRITISH AND
FOREIGN SHIPPING.

SIR,

I am instructed to direct your attention to the Rules for Iron Ships, Sec. 11, prescribing that "the overlaps of plating, where double rivetting is required, are not to be less in breadth than five times the diameter of the rivets; and where single rivetting is admitted, the overlaps not to be less than three diameters of the rivets;" and to express the Committee's desire that you will take care that this regulation is in all cases carried strictly into effect.

The Committee, having reason to believe that the practice of fitting Ships with shifting beams is greatly on the increase, request that you will report specially in all such cases, and state explicitly what compensation is made to secure their efficiency.

With reference to the Rule, requiring that "every third year Iron Ships shall be specially surveyed in dry dock or laid on blocks, with both surfaces of outside plating exposed," as applicable to Ships where the inner surface of the bottom plating is coated with cement or asphalte: it has been determined that, in such cases, a sufficient quantity of ceiling shall be removed to enable the coating to be carefully inspected and tested by beating or chipping, and if it be found sound and good, and adhering sufficiently to the iron, then the removal of such coating may be dispensed with; you will, therefore, be governed herein accordingly.

I am,

Sir,

Your obedient Servant,

GEORGE B. SEYFANG,

Secretary.

2, White Lion Court, Cornhill, London, E.C.,
17th April, 1862.

LLOYD'S REGISTER OF BRITISH AND FOREIGN SHIPPING.

RULES AND REGULATIONS.

NOTICE is hereby given that in pursuance of Resolutions passed by the Committee on the 20th of March, the following Rules have been amended, viz. :—

Sec. 39, so as to require that ships, the length of which exceeds eight times, but is under nine times their depth, shall have the same longitudinal strengthening as is prescribed for ships the length of which exceeds five times their breadth.

And that where the length exceeds nine times, and is under ten times their depth, they be subjected to the same requirements as those which exceed in length six times their breadth.

And in cases where the length exceeds ten times the depth, the Builders or Owners are to submit, for the Committee's approval, their plans for giving the Vessel the necessary strength longitudinally.

Sec. 60. Ships classed originally A for 7 years and under, which may be classed A in red, under the second clause of the Rule, and which have been doubled, will be allowed exemption from Special re-survey for a period not exceeding five years, or for the term (if less than five years) for which they were classed originally.

By order of the Committee,

GEORGE B. SEYFANG,

Secretary.

THE JOURNAL OF THE AMERICAN MEDICAL ASSOCIATION
PUBLISHED WEEKLY
CHICAGO, ILL., U.S.A.

ORIGINAL ARTICLES

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